



# DOWNTOWN STREETScape IMPROVEMENT PLAN

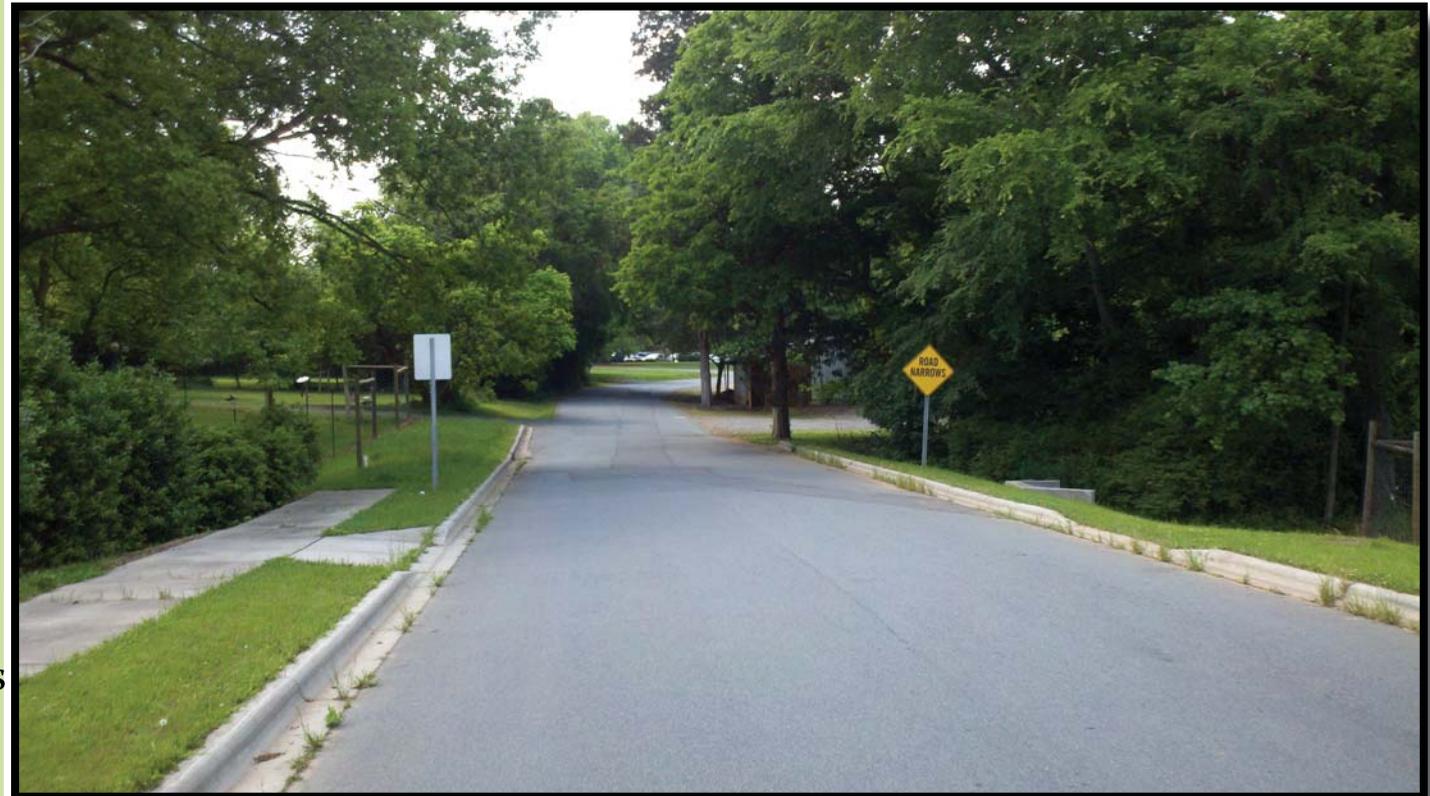
## 2015



## TABLE OF CONTENTS

### Pages

2.....	Plan Purpose
3.....	Objectives and How to use these guidelines
4.....	Public Input
5.....	Downtown Limits & Districts Maps
6.....	Downtown Street Types
7.....	Typical Arterial-Urban Commercial Street
8.....	Typical Arterial-General Street
9.....	Typical Downtown Collector Street
10.....	Typical Local Street
11.....	Typical Urban Residential Street
12.....	Typical Unique Street
13.....	Street Cross-sections of Interest (Map)
14-43...	Downtown Cross-sections and Photo Renderings
44.....	Bike and pedestrian paths through Downtown
45.....	Concept for pedestrian path through parking area between Trade Street and Ames Street
46.....	Concepts for John Street widening

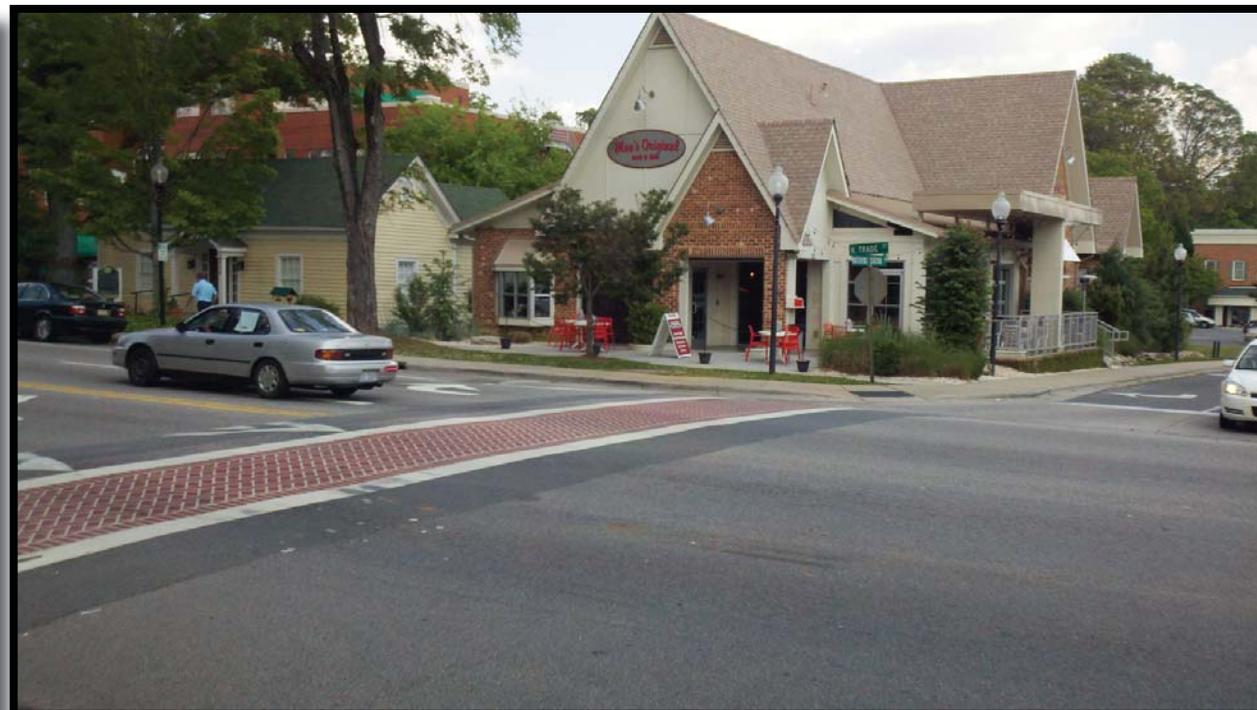


West Charles Street

# DOWNTOWN STREETScape IMPROVEMENT PLAN



Matthews Town Hall and Public Library



Pedestrian Crossing at Matthews Station Street and North Trade Street

## Purpose

This guide serves as an addendum to and expansion of the existing Town of Matthews Downtown Plan as adopted by the Matthews Board of Commissioners January 7, 2013 and update the 1998 Streetscape Plan. Existing and proposed street cross sections and streetscape improvements are illustrated through sketches and photo edits of before and after images. The proposed streetscape improvements for the downtown area of Matthews include any combination of the following elements: proposed infill development, curb and gutter, sidewalks, multi-use paths, curb ramps, bicycle racks, street lighting, way-finding signage, and landscaping. Dimensions for streets, planting strips, sidewalks and building setbacks are provided. Some streets reviewed in this document are controlled by the Town and others may be controlled by the North Carolina Department of Transportation. It is recommended that property owners and businesses located within Downtown refer to the Matthews Downtown Plan and other Town planning documents for additional advice and direction on development issues.

The recommended style for all Downtown development should reflect basic principles of geometry and form while complimenting the existing rural North Carolina Piedmont vernacular of the late 19th and early 20th centuries. Streetscape elements such as benches, street lights, sign poles, and landscaping should enhance, not conflict with those examples found within the Downtown Core District.

## Objectives

This plan provides:

- An inventory of existing street-cross sections, transportation infrastructure, and land uses,
- A map indicating street types within the Downtown Area,
- Graphic examples of proposed street cross-sections,
- Photo-realistic before and after representations of how new streets should be developed/ redeveloped illustrating how buildings, sidewalks, and other streetscape elements should interact.

## How to use these guidelines

Streetscape cross-section Guidelines help implement the recommendations in the approved and adopted Downtown Plan. They provide information on how plan recommendations and zoning code requirements can be met, the area or district context for individual sites, and ideas about best practices in building and site design.

The planning process is structured in a hierarchy of decisions:

- The Downtown Plan recommendations provide the vision for a specific area,
- Unified Development Ordinance and other codes establish standards and regulations for development,
- Streetscape Cross-section Guidelines provide inspiration and suggestions to fulfill the Downtown Plan's vision, and serve as a problem-solving tool.

The cross-section guidelines are developed through work with property owners, residents, institutions, interest groups, and Town Staff. They are approved by the Town Board for use by planning staff in developing and evaluating proposed building projects and other applications. They will be revised and updated as necessary. With the exception of street standards and other specific recommendations included in the Downtown Plan, the guidelines are not regulations that mandate specific forms and locations for buildings and open space. They illustrate how plan recommendations and principles might be met, and encourage an attractive and successful



Traditional Streetscape on North Trade Street (Downtown Core District)



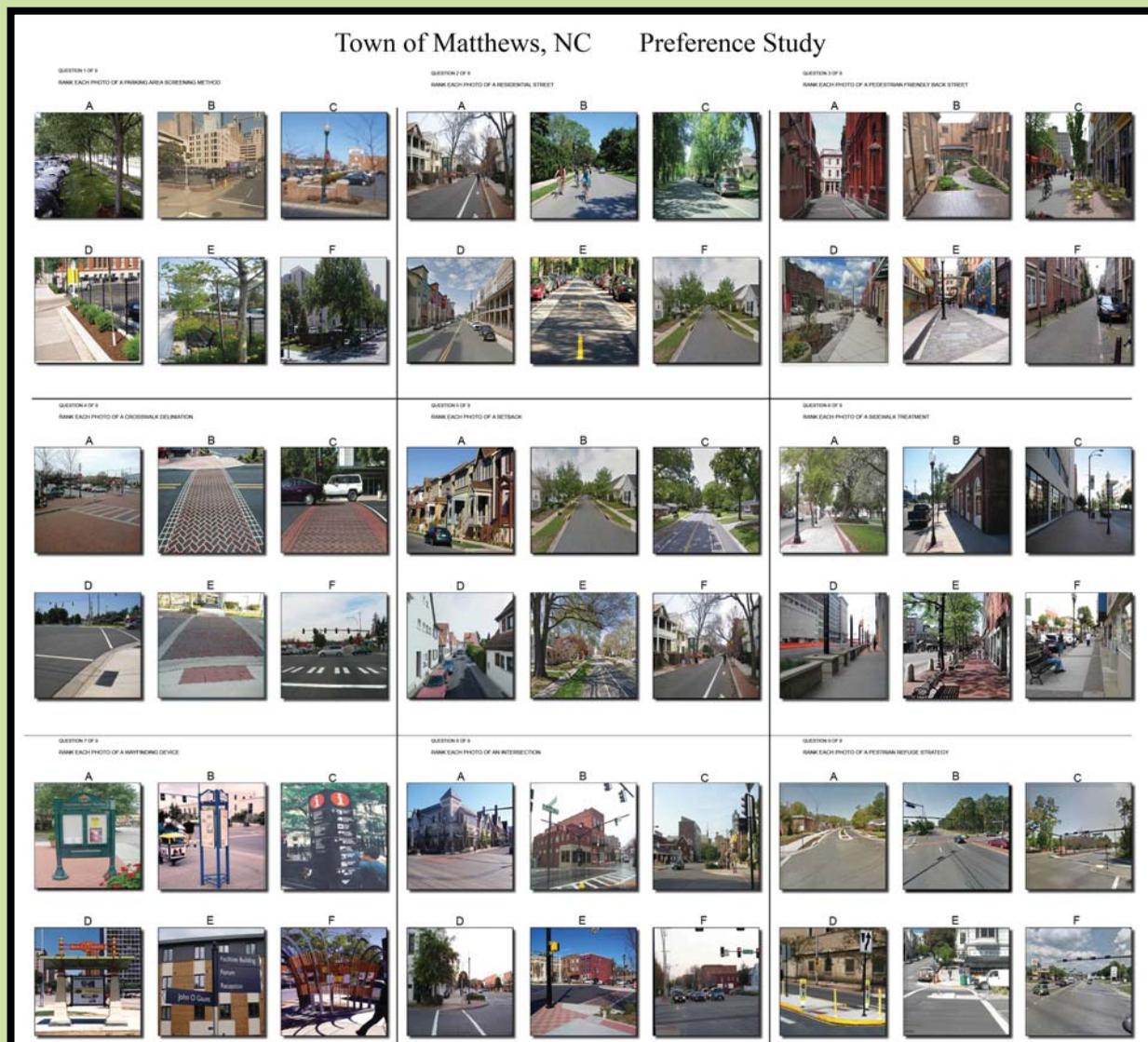
Contemporary Streetscape on North Trade Street (North End District)

# DOWNTOWN STREETScape IMPROVEMENT PLAN

## Public Input

A Visual Preference Survey was conducted at the Food Truck Friday event held on September 18th, 2015. Public outreach included the preference exercise, review of proposed streetscape recommendations and open discussion. The survey was made available online via a link on the Town's webpage from September 18th thru October 6th. The survey consisted of different groups of images depicting potential streetscape treatments or urban environments. Participants were asked to rate each image in a given group according to their preference (1 being most preferred - 6 least preferred).

The participants' input can be used to make inferences about visual preference as it relates to certain elements of streetscape design. Prioritized results of the survey participant's responses start with the highest preference and end with the least preferred image. There were 50 volunteer participants that provided input about their preferences. The results of the survey may be found below:



## Results:

Q1: Preferences for “Parking Area Screening” method was as follows: A, E, F, C, D, B  
Respondents favored wide, treed, green spaces over hardscape such as walls.

Q2: Preferences for “Residential Street” was as follows: C, B, F, E, A, D  
Respondents favored streets that both accommodated pedestrians and provided vegetative screening between structures and the flow of traffic.

Q3: Preferences of a “Friendly Back Street” was as follows: C, B, D, E, A, F  
Respondents favored back streets that incorporated vegetation and that offered places of respite.

Q4: Preferences for “Cross-walk Delineation” was as follows: C, B, A, E, F, D  
Respondents preferred crosswalks with high contrasting colors and brick patterns over traditional, standard white striped designs.

Q5: Preferences for a “Setback” was as follows: E, C, B, A, F, D  
Respondents preferred wide (20'-40'), suburban yard type setbacks that accommodate space for pedestrian paths and trees.

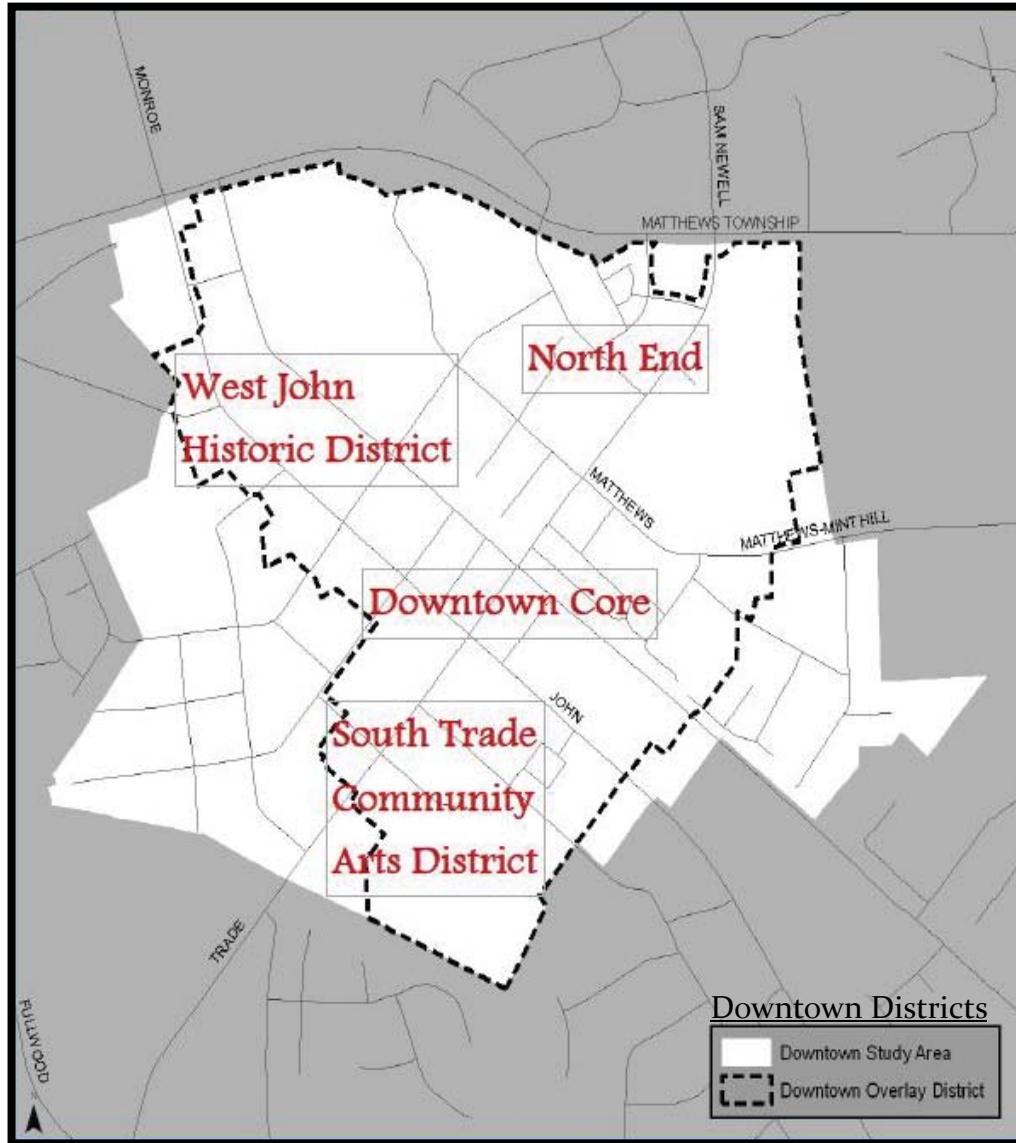
Q6: Preferences for “Sidewalk Treatment” was as follows: A, E, F, C, B, D  
Respondents preferred brick accented, wide sidewalks that provide pedestrian amenities.

Q7: Preferences for “Wayfinding Devices” was as follows: A, E, B, D, C, F  
Respondents preferred conservative, unobtrusive, traditional type wayfinding signage over larger, pretentious, landmark type signage.

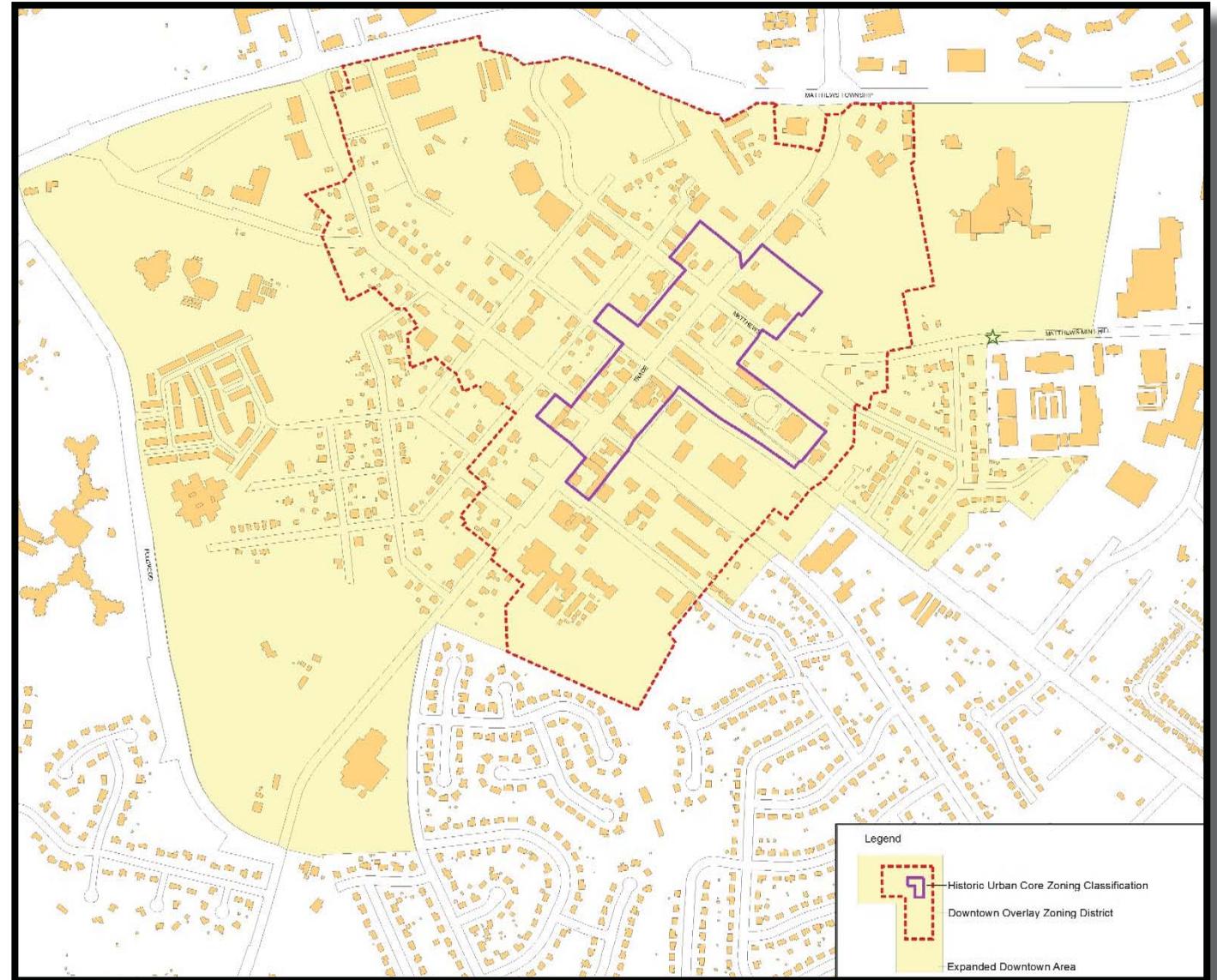
Q8: Preferences for “Intersections” was as follows: E, A, D, B, C, F  
Respondents favored decorative paving (distinct colors and materials), space for landscaping, and pedestrian oriented amenities. The look would be more “village” and less “city”.

Q9: Preferences for a “Pedestrian Refuge Strategy” was as follows: C, A, D, B, E, F  
Respondents favored those street crossings that provided a pedestrian refuge island integrated within the street median. Preference was for highly visible waiting areas within vegetated, well-marked, pedestrian oriented locations.

# DOWNTOWN STREETScape IMPROVEMENT PLAN



Downtown Districts

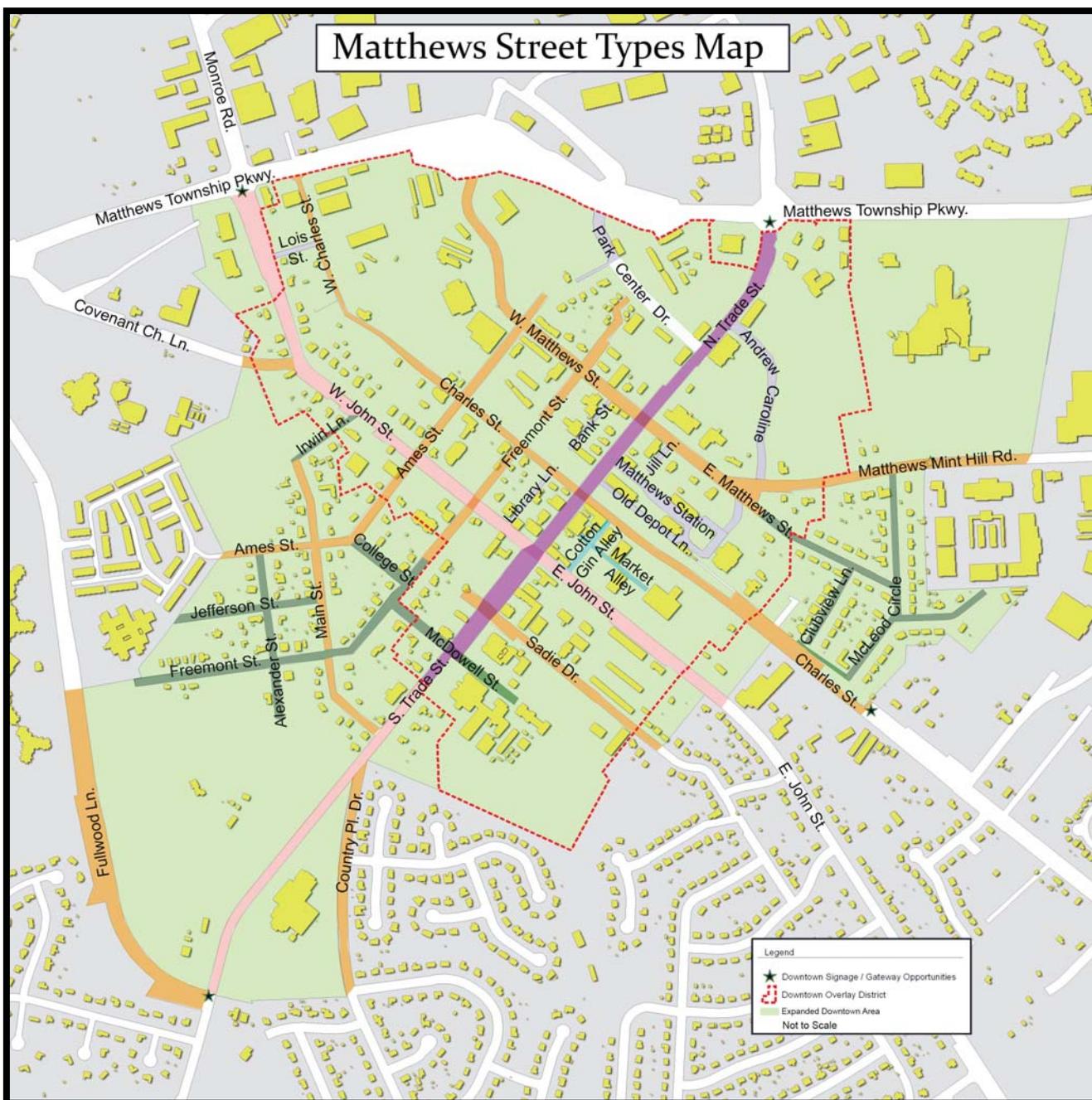


Map highlighting the Historic Urban Core, Downtown Overlay and Expanded Downtown Areas

## Downtown Area

The recommendations presented in this guide refer to areas within the Downtown area as defined by the Town of Matthews Planning department. The above map delineates the area considered within the Downtown area.

# DOWNTOWN STREETScape IMPROVEMENT PLAN



## Street Types Purpose

The Street Types Map displays corridors where typical minimum streetscape cross-sections are recommended. Streetscape design features, and building-to-street relationships will support the development of Downtown as a focus of the community and as pedestrian-oriented, multi-modal transportation friendly area. Standards of design for each type of street are graphically illustrated within this document. Specific locations where variations to the typical minimum cross-sections are recommended can be identified via the Cross-section Locations Map on page 11.

## Street Types

These Street Type designations will ensure consistent application of appropriate treatments based on mixes of uses and levels of intensity throughout the different Downtown districts.

**Arterial Urban Commercial** - Part of South Trade Street & North Trade Street

**Arterial General** - East & West John Street, Part of South Trade Street

**Downtown Collector Street** - Charles Street, Matthews Street, Country Place Drive, Fullwood Lane, Main Street, Sadie Drive, Part of Freemont Street, Ames Street, Matthews-Mint Hill Road, Covenant Church Lane

**Local Street** - Matthews Station Street, Old Depot Lane, Bank Street, Library Lane, Park Center Drive, Jill Lane, Andrew Caroline, Lois Street, Clubview Lane

**Urban Residential Street** - Irwin Lane, College Street, McDowell Street, McLeod Circle, Jefferson Street, Alexander Street, Part of East Matthews Street, Part of Freemont Street

**Unique Street** - Market Alley, Cotton Gin Alley

# DOWNTOWN STREETSCAPE IMPROVEMENT PLAN

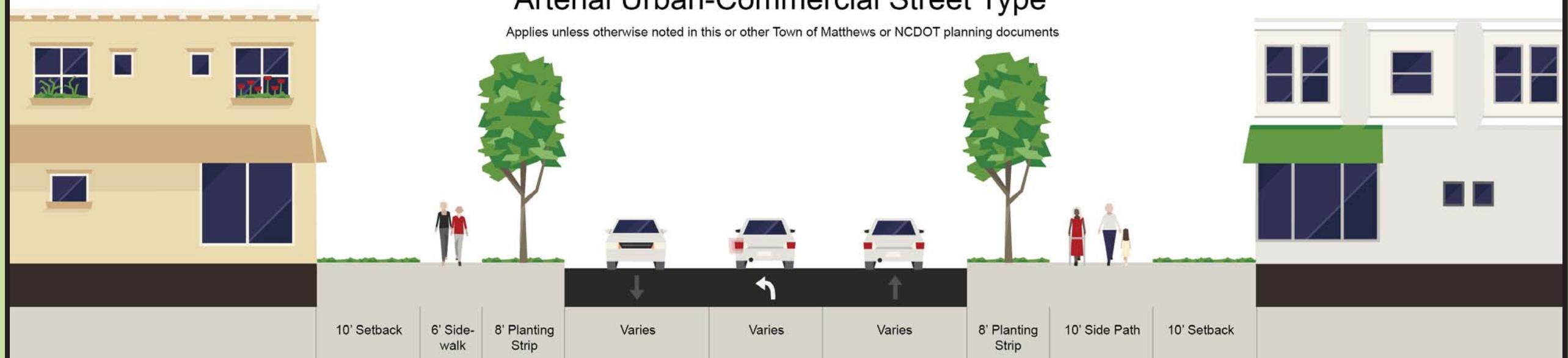
## Arterial - Urban Commercial

For the **Arterial - Urban Commercial** Street Type, all the following minimum Street Type Standards shall be met unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with the North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Unified Development Ordinance.
2. There shall be a clear, accessible sidewalk of a minimum of 6 feet in a 14'-24' zone that includes the curb, planting strip and building setback. Variations on proposed designs including sidewalk, or planting strips must be approved by the Town of Matthews. Appurtenances can protrude 1 foot into the walk width and may include typical streetscape furnishings as approved by the Town of Matthews.
3. There shall be a planting strip between 0'-8' in width provided between the walking zone and travel lane. This amenity zone may consist of street trees, street lighting, landscaping and/or seating.
4. On-street parking shall be permitted where space exists and where prior approval by the Town of Matthews and NCDOT (where required) has been obtained.
5. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
6. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.

### Typical Minimum Streetscape Cross-Section for Arterial Urban-Commercial Street Type

Applies unless otherwise noted in this or other Town of Matthews or NCDOT planning documents

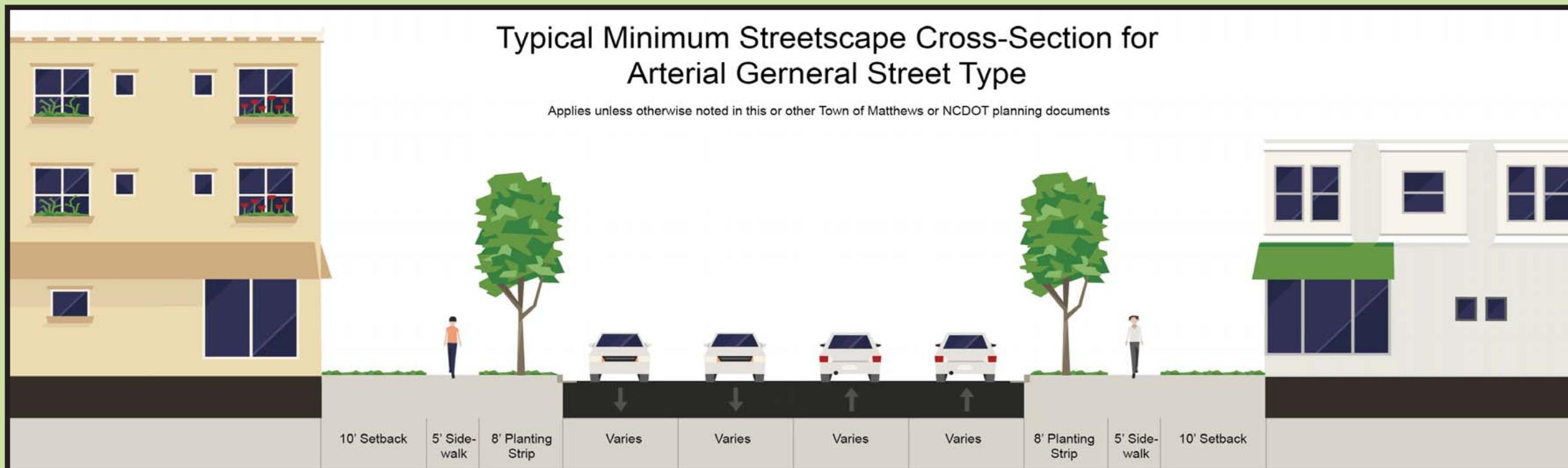


# DOWNTOWN STREETScape IMPROVEMENT PLAN

## Arterial - General

For the **Arterial - General** Street Type, all the following minimum Street Type Standards shall be met unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Unified Development Ordinance.
2. There shall be a clear, accessible sidewalk of a minimum of 5 feet in an 23-foot zone that includes the curb, planting strip and building setback. Appurtenances can protrude 1 foot into the walk width and may include typical streetscape lighting and furnishings as approved by the Town of Matthews.
3. There shall be a 8-foot planting strip provided between the sidewalk and travel lane. This amenity zone may consist of street trees, street lighting, landscaping and/or seating.
4. No on-street parking shall be permitted. Parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules for more details.
5. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
6. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.



## Downtown Collector Street

For the **Downtown Collector** Street Type, all the following minimum Street Type Standards shall be met unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Development Codes.
2. There shall be a clear, accessible sidewalk of a minimum of 5 feet in an 30-foot zone that includes the curb, planting strip and building setback. Appurtenances can protrude 1 foot into the walk width and may include typical streetscape lighting and furnishings as approved by the Town of Matthews.
3. There shall be a 4-foot planting strip provided between the sidewalk and travel lane. This amenity zone may consist of street trees, street lighting, landscaping and/or seating.
4. On Street Parking shall be provided on at least one side of the Street with a Bicycle Lane on the opposite side of the Street where permitted by Town Ordinances. Parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules for more details.
5. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
6. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.

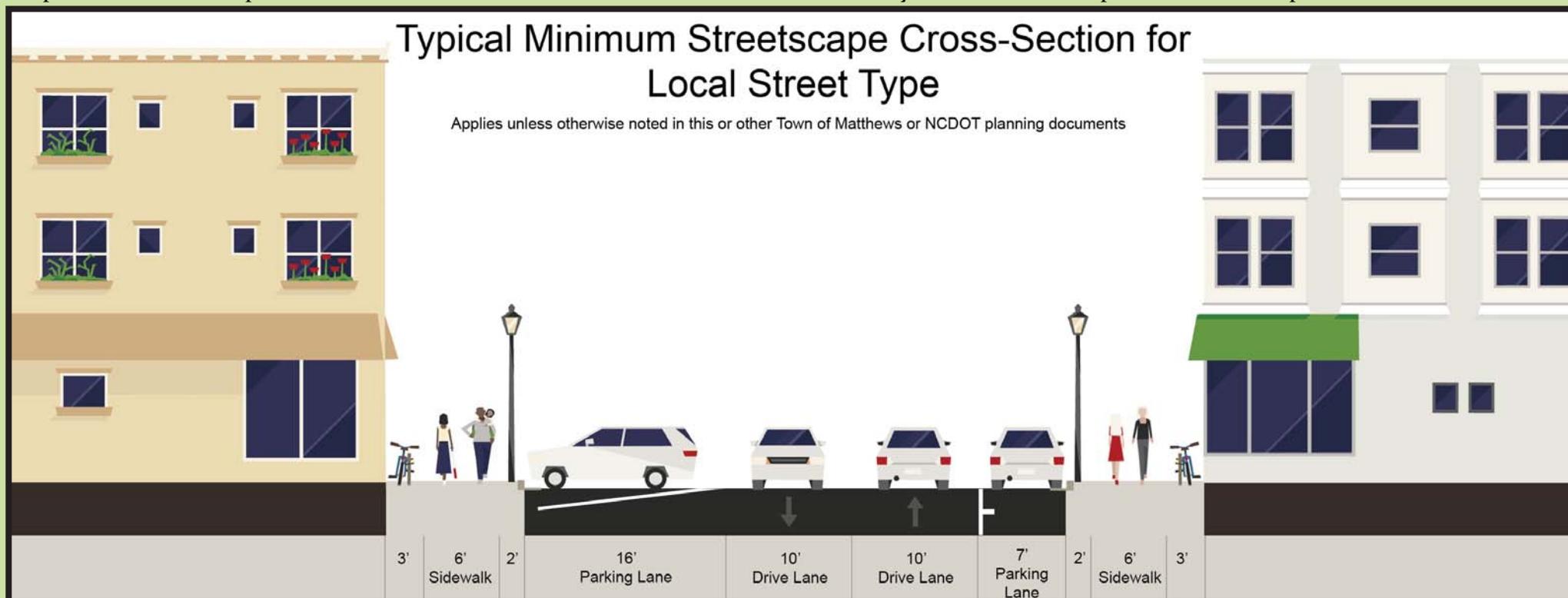


# DOWNTOWN STREETScape IMPROVEMENT PLAN

## Local Street

For the Local Street Type, all the following minimum Street Type Standards shall be met unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Unified Development Ordinance.
2. There shall be a clear, accessible sidewalk of a minimum of 5 feet in an 11-foot zone that includes the curb, planting strip and building setback. Appurtenances can protrude 1 foot into the walk width and may include typical streetscape lighting and furnishings as approved by the Town of Matthews.
3. There shall be a 3-foot pedestrian amenity zone provided between the sidewalk and building facade. This amenity zone may consist of bike racks, seating, removable planters and temporary signage as approved by the Town of Matthews.
4. On street parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules for more details.
5. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
6. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.



# DOWNTOWN STREETSCAPE IMPROVEMENT PLAN

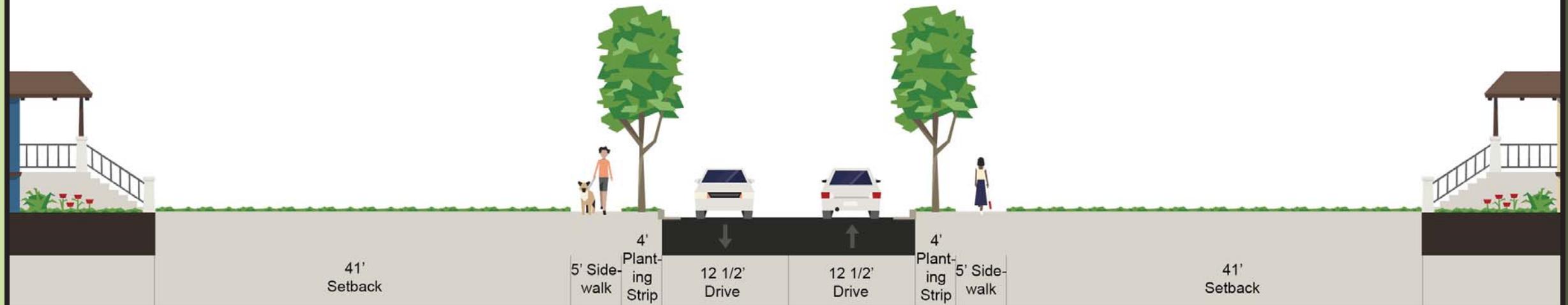
## Urban Residential Street

For the Local Street Type, all the following minimum Street Type Standards shall be met unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Development Codes.
2. There shall be a clear, accessible sidewalk of a minimum of 5 feet in an 11-foot zone that includes the curb, planting strip and building setback. Appurtenances can protrude 1 foot into the walk width and may include typical streetscape lighting and furnishings as approved by the Town of Matthews.
3. There shall be a 3-foot bike rack zone provided between the sidewalk and building facade. This amenity zone may consist of bike racks, seating, removable planters and temporary signage as approved by the Town of Matthews.
4. Parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules for more details.
5. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
6. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.

## Typical Minimum Streetscape Cross-Section for Urban Residential Street Type

Applies unless otherwise noted in this or other Town of Matthews or NCDOT planning documents

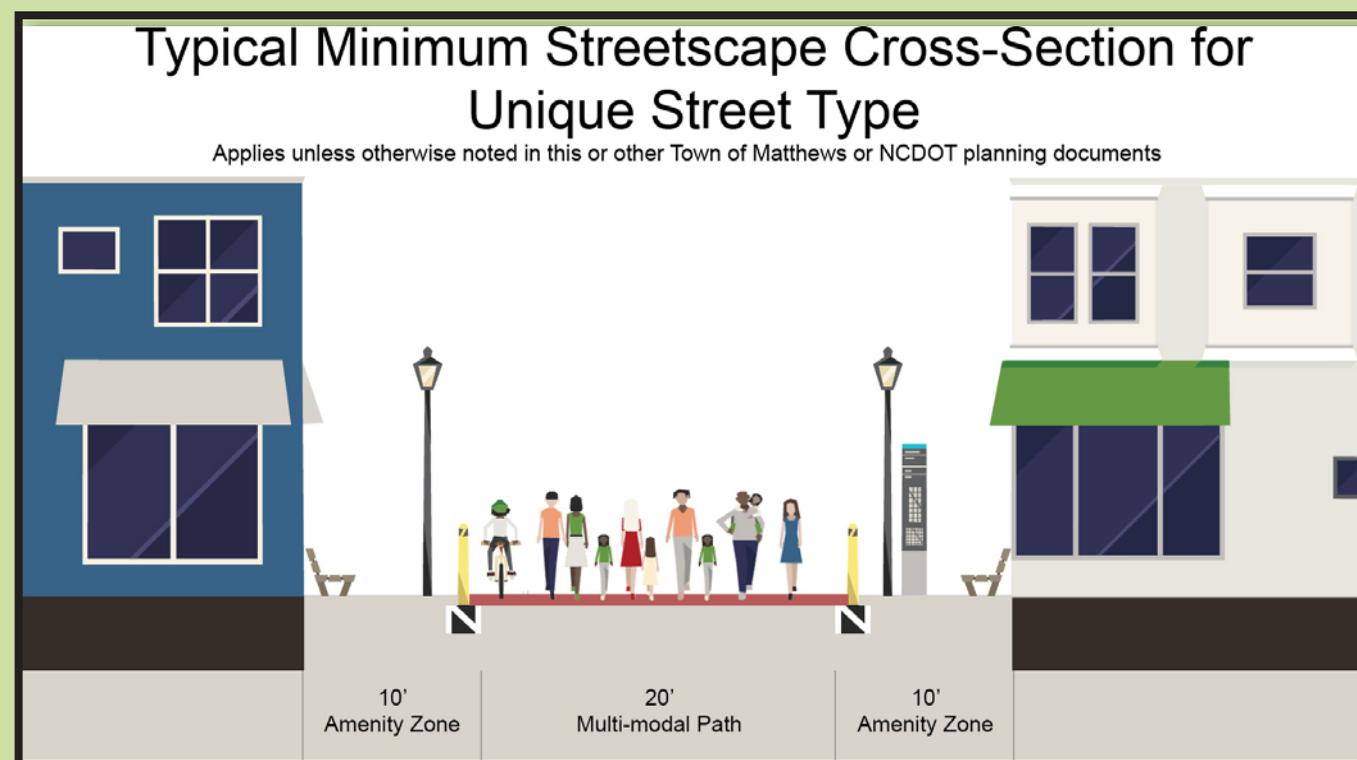


# DOWNTOWN STREETScape IMPROVEMENT PLAN

## Unique Street

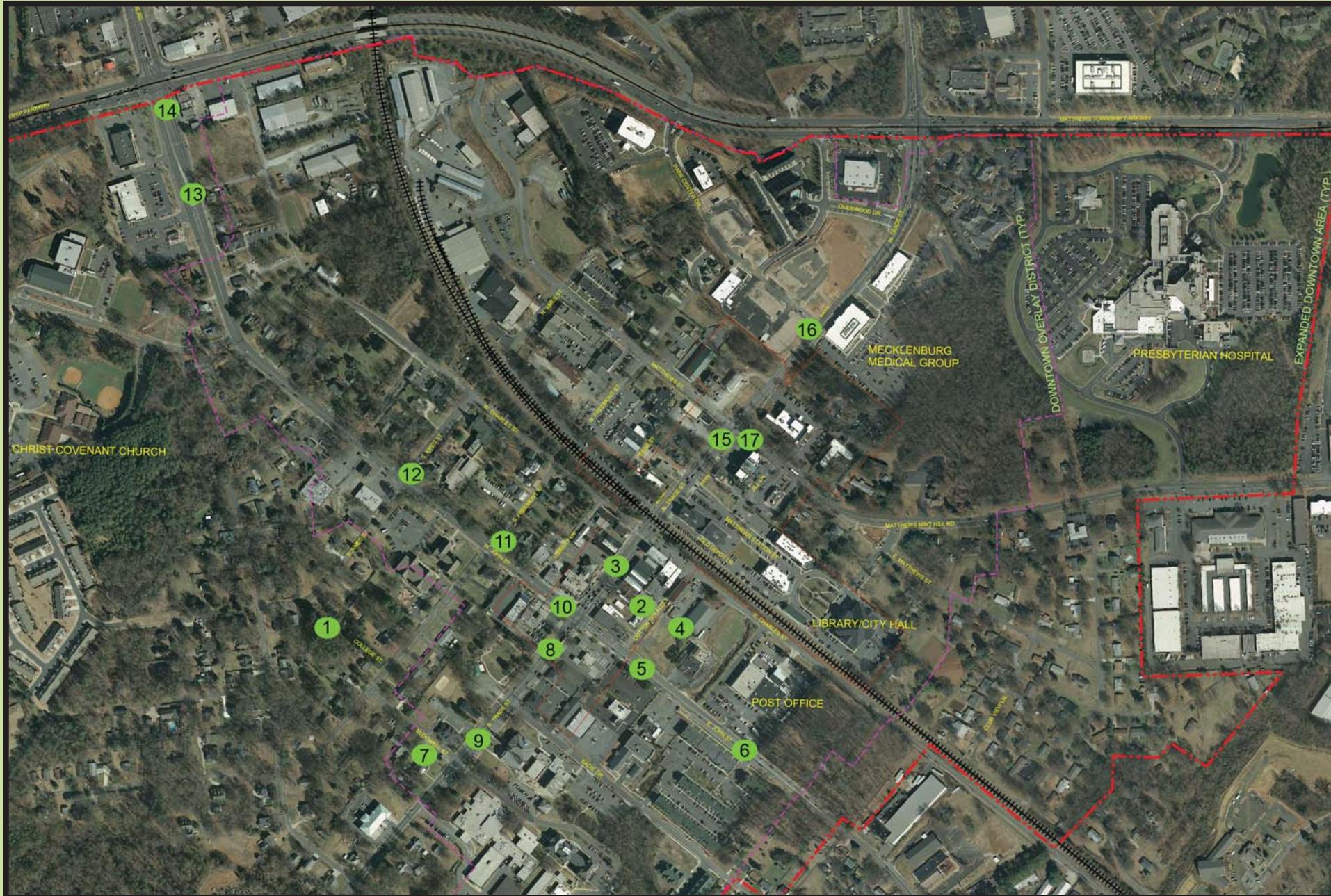
For the Local Street Type, all the following minimum Street Type Standards shall be met unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Unified Development Ordinance.
2. There shall be a clear, accessible multi-modal path of a minimum of 20 feet in an 40-foot zone. Limited vehicular traffic may be permissible per Town of Matthews and should be limited to speed limits of 5 m.p.h. or less.
3. The 10' amenity zone may consist of bike racks, seating, removable planters and temporary signage as approved by the Town of Matthews.
4. Parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules for more details.
5. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.



# DOWNTOWN STREETScape IMPROVEMENT PLAN

## Street Cross Section Inventory



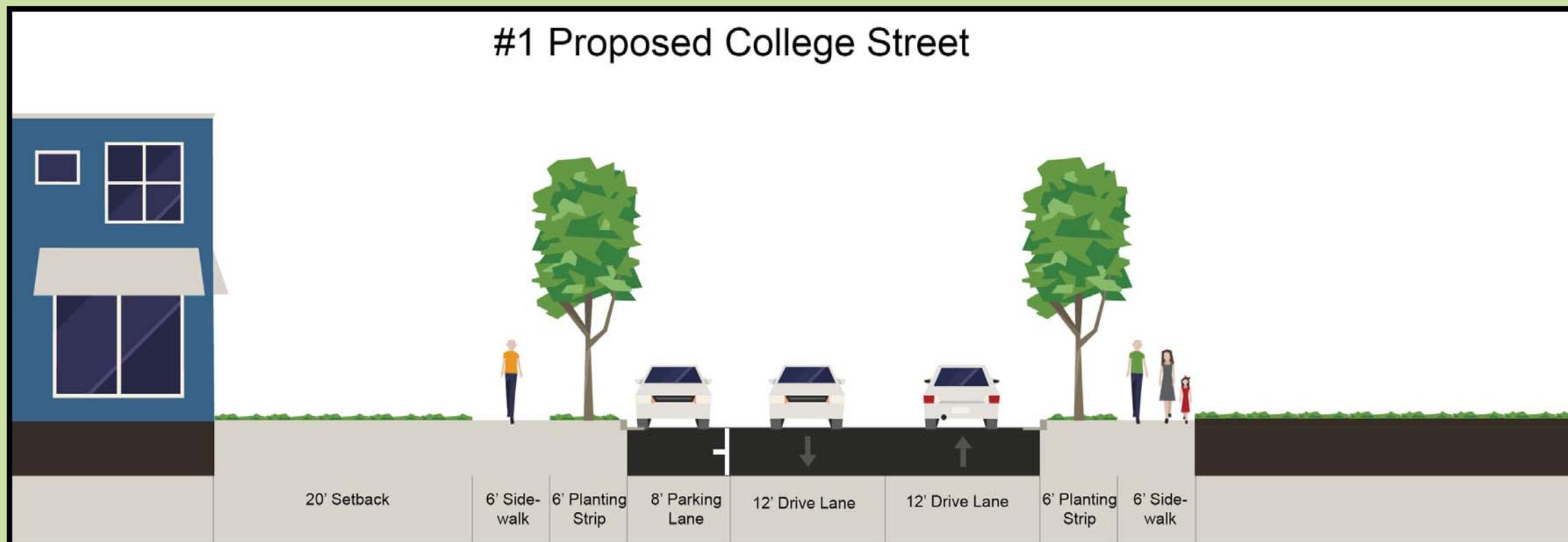
Please refer to the street types map on page six (6) of this document for standard recommended cross-sections of most Downtown streets. Locations/corridors with cross-section recommendations that deviate from the typical may be identified via this map.

### Cross-section Locations

1. College Street
2. Cotton Gin Alley (John St. to Mid-Block)  
Cotton Gin Alley (Mid-Block to Charles St.)
3. 100 Block of N. Trade
4. Market Alley
5. E. John (BB&T to Trade)
6. E. John (Buckley Way to BB&T)
7. McDowell St.
8. S. Trade at W. John
9. S. Trade at Baptist Church
10. W. John at Trade (options A & B)
11. W. John near Freemont St.
12. W. John near Ames St.
13. W. John near Lois St.
14. W. John at Matthews Township Pkwy.
15. N. Trade at Matthew St.
16. N. Trade at Park Center Dr.
17. E. Matthews at Trade St.

# DOWNTOWN STREETScape IMPROVEMENT PLAN

## (Proposed) College Street (Looking North West)

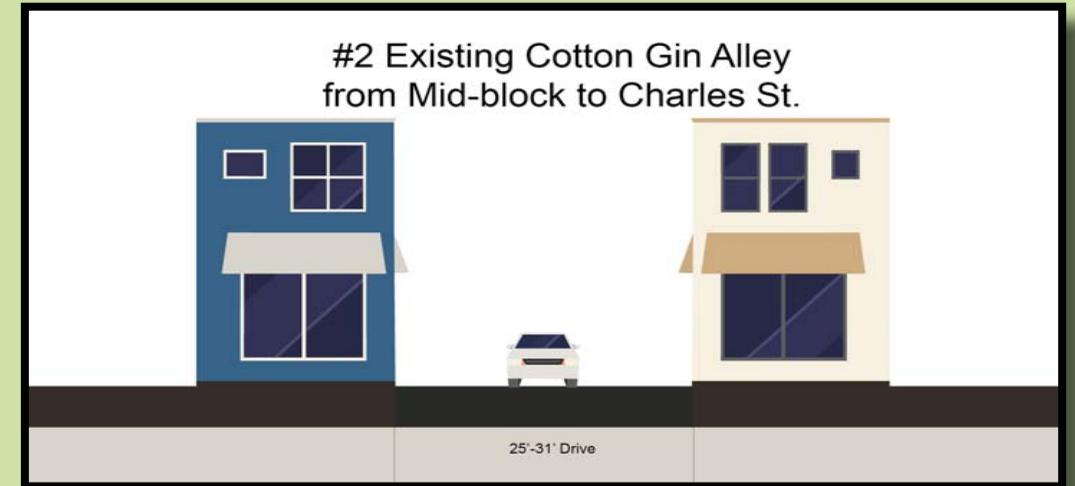
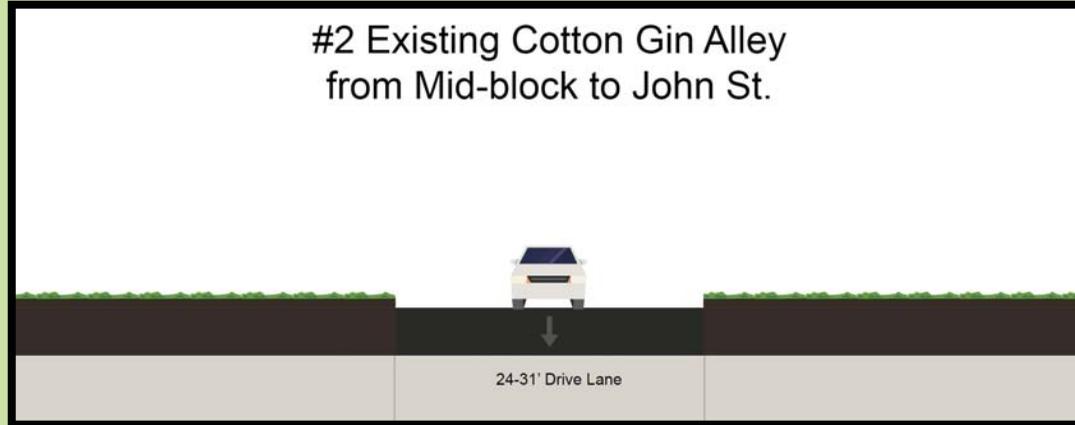


College Street shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Unified Development Ordinance.
2. There shall be a clear, accessible sidewalk on both sides of the street of a minimum of 6 feet in an 40-foot zone between the face of any building and the face of the street or edge of pavement.
3. Parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules for more details.
4. Any Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.

# DOWNTOWN STREETSCAPE IMPROVEMENT PLAN

## Cotton Gin Alley



Cotton Gin Alley shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Development Codes.
2. There shall be a clear, accessible sidewalk on both sides of the street of a minimum of 6 feet in width.
3. Parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules for more details.
4. Any Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.

# DOWNTOWN STREETScape IMPROVEMENT PLAN

## 100 Block of North Trade Between John and Charles Streets (Looking North)

The proposed changes to the 100 block of North Trade Street predominately consist of moving the face of curb on the west side of the street out 11' towards the center of the street thus eliminating the angled parking while establishing parallel parking in its place. This would affect the north half of this block up to the point where the existing parallel parking. This existing parallel parking shall remain. The advantage of this proposed change is that a wider pedestrian area is established. This concept shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

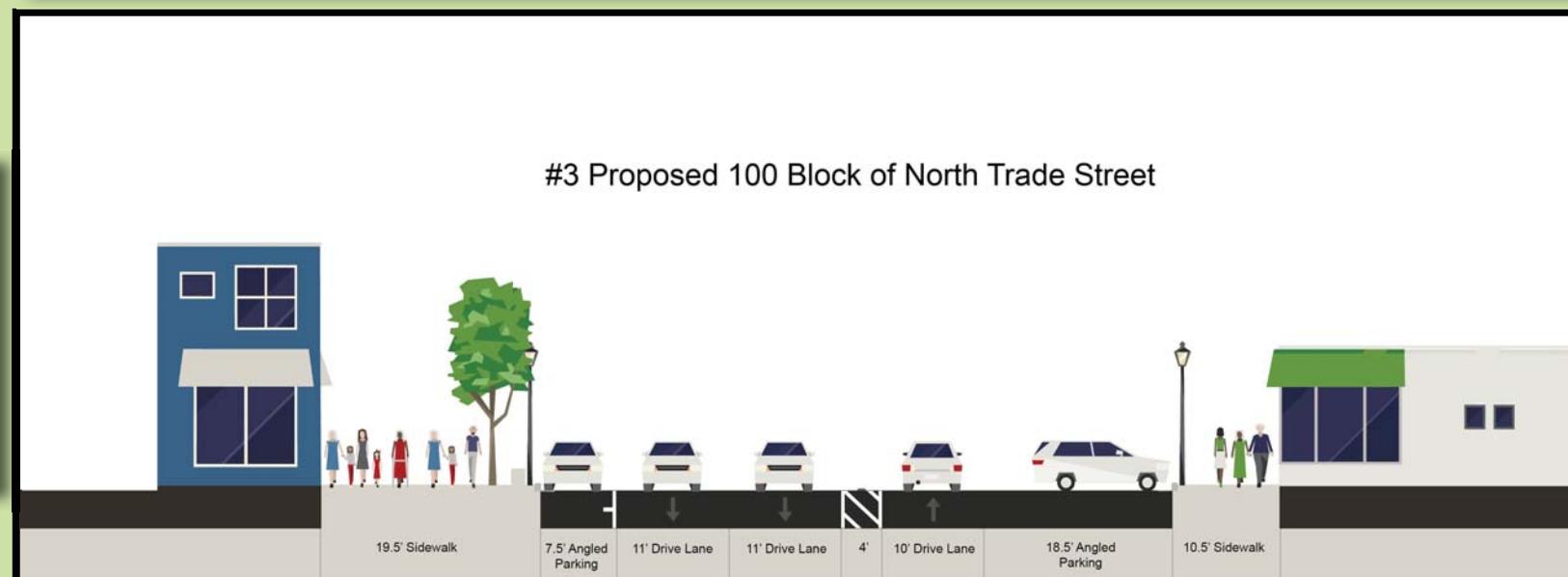
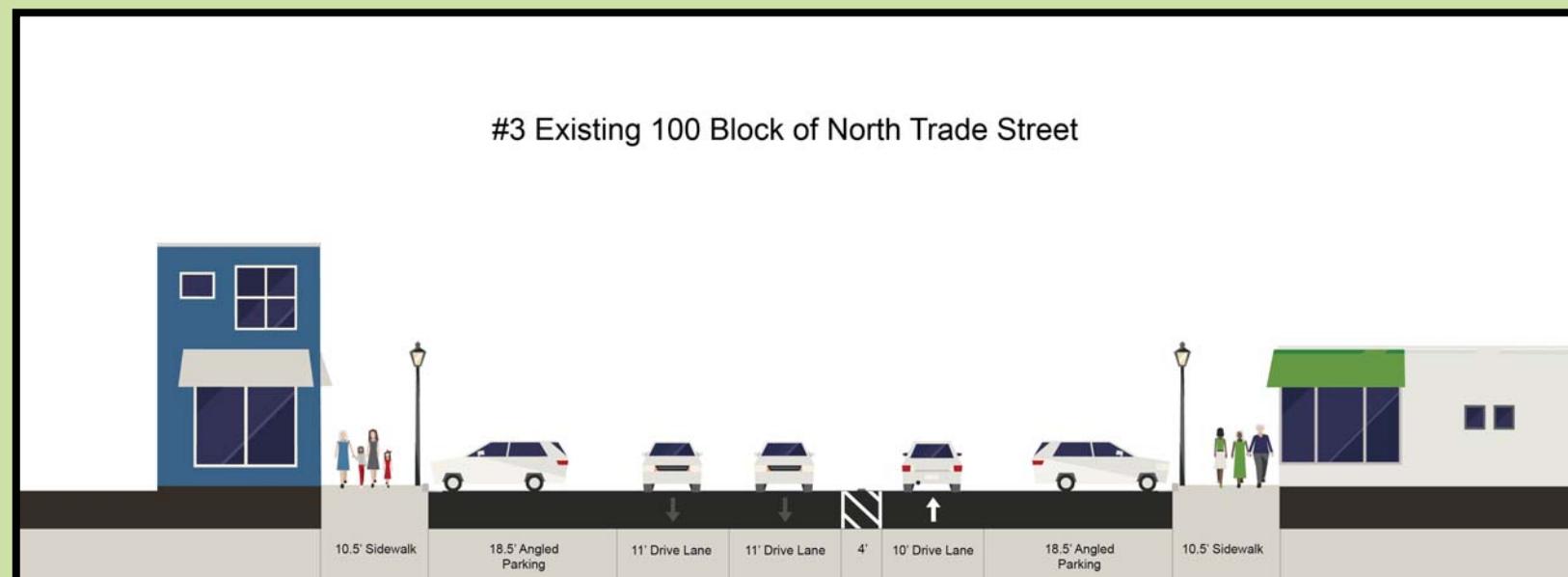
2. There shall be a clear, accessible path on both sides of the street that meets A.D.A. standards.
3. Parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules for more details.
4. Any Street lamp lighting should be located within 4 feet of the face of curb and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.



BEFORE



AFTER



# DOWNTOWN STREETSCAPE IMPROVEMENT PLAN

## 100 Block of North Trade Between John and Charles Streets (Looking South)

**BEFORE**



**AFTER**



# DOWNTOWN STREETScape IMPROVEMENT PLAN

## (Proposed) Market Alley



The proposed Market Alley shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. The intent of the proposed Market Alley should be to provide a mixed-use, multi-modal, pedestrian oriented space. Through traffic should be limited by speed and traffic calming techniques.
2. There shall be a clear, accessible multi-modal path of a minimum of 20 feet in an 40-foot zone. Limited vehicular traffic may be permissible per Town of Matthews and should be limited to speed limits of 5 m.p.h. or less.
3. The 10' amenity zone may consist of bike racks, seating, removable planters and temporary signage as approved by the Town of Matthews.
4. Parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules for more details.
5. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.

# DOWNTOWN STREETScape IMPROVEMENT PLAN

## Proposed Market Alley

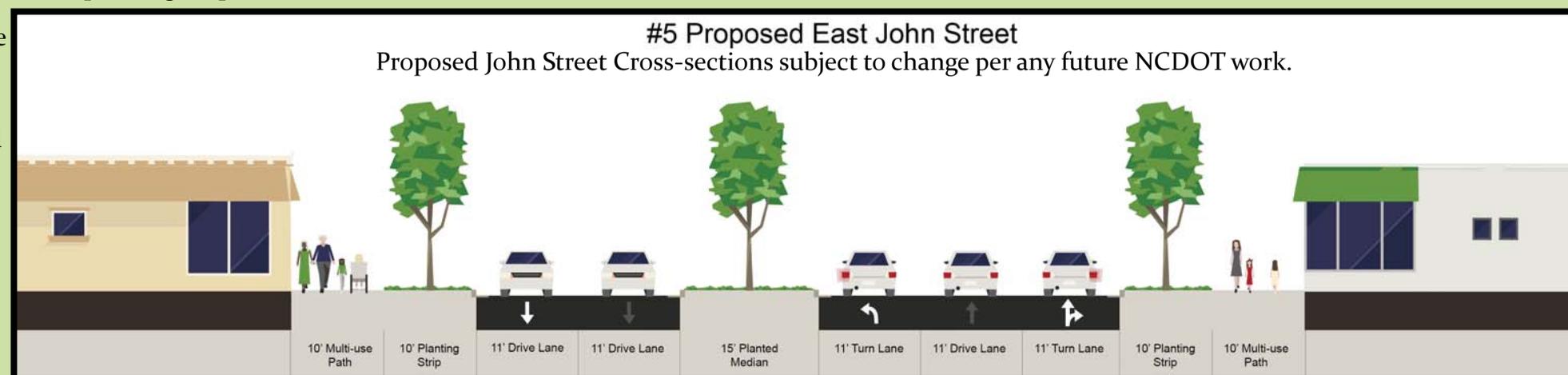
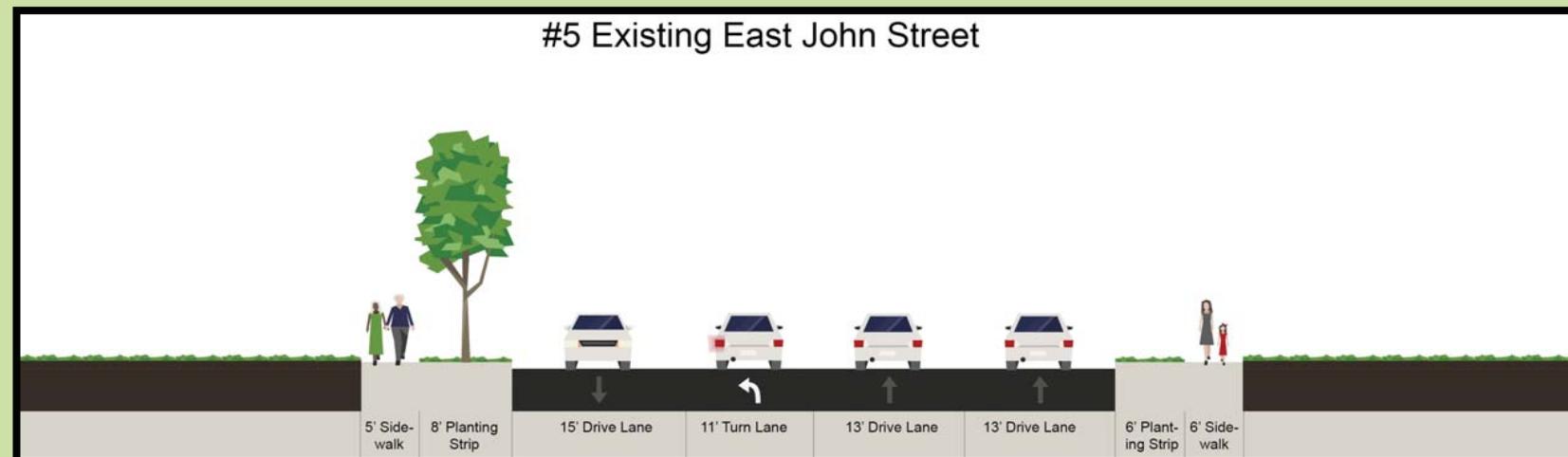


# DOWNTOWN STREETScape IMPROVEMENT PLAN

## Typical East John Street from BB&T to Trade Street (Looking West)

The proposed typical East John Street cross section shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Development Codes.
2. There shall be a clear, accessible sidewalk of a minimum of 5 feet in an 23-foot zone that includes the curb, planting strip and building setback. Appurtenances can protrude 1 foot into the walk width and may include typical streetscape lighting and furnishings as approved by the Town of Matthews.
3. There shall be a 8-foot planting strip provided between the sidewalk and travel lane. This amenity zone may consist of street trees, street lighting, landscaping and/or seating.
4. No on-street parking shall be permitted. Parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules or more details.
5. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
6. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.

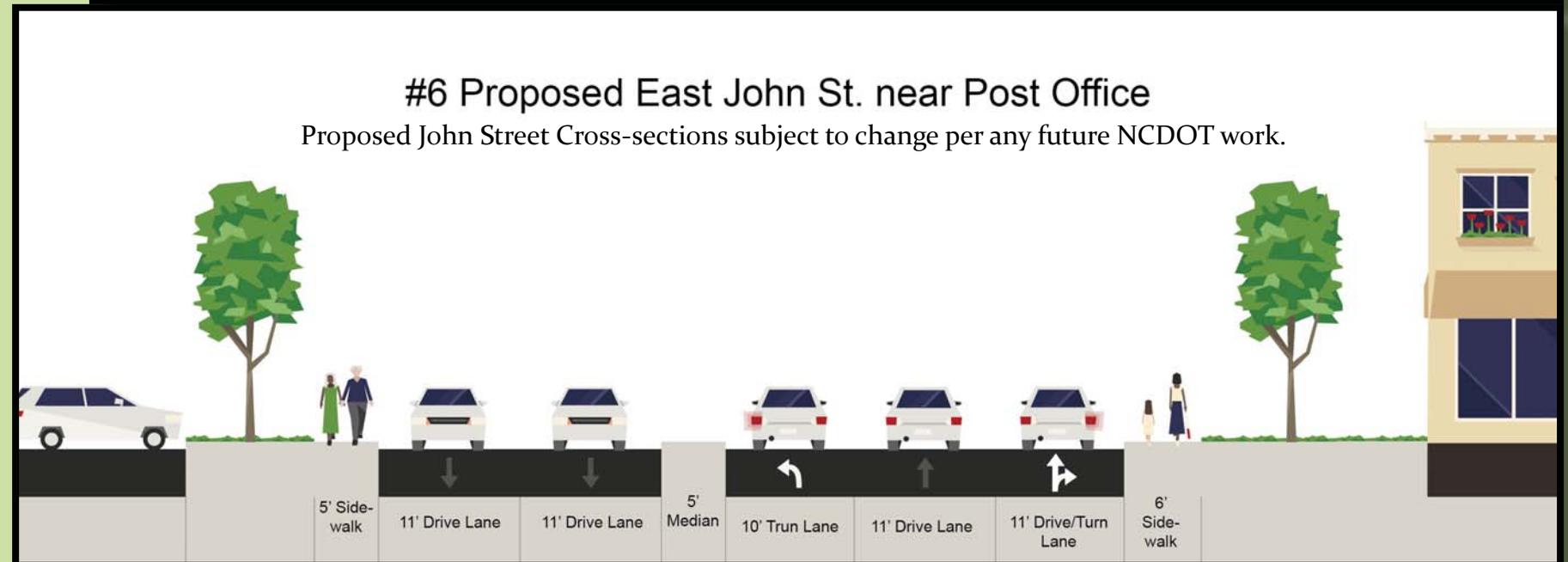
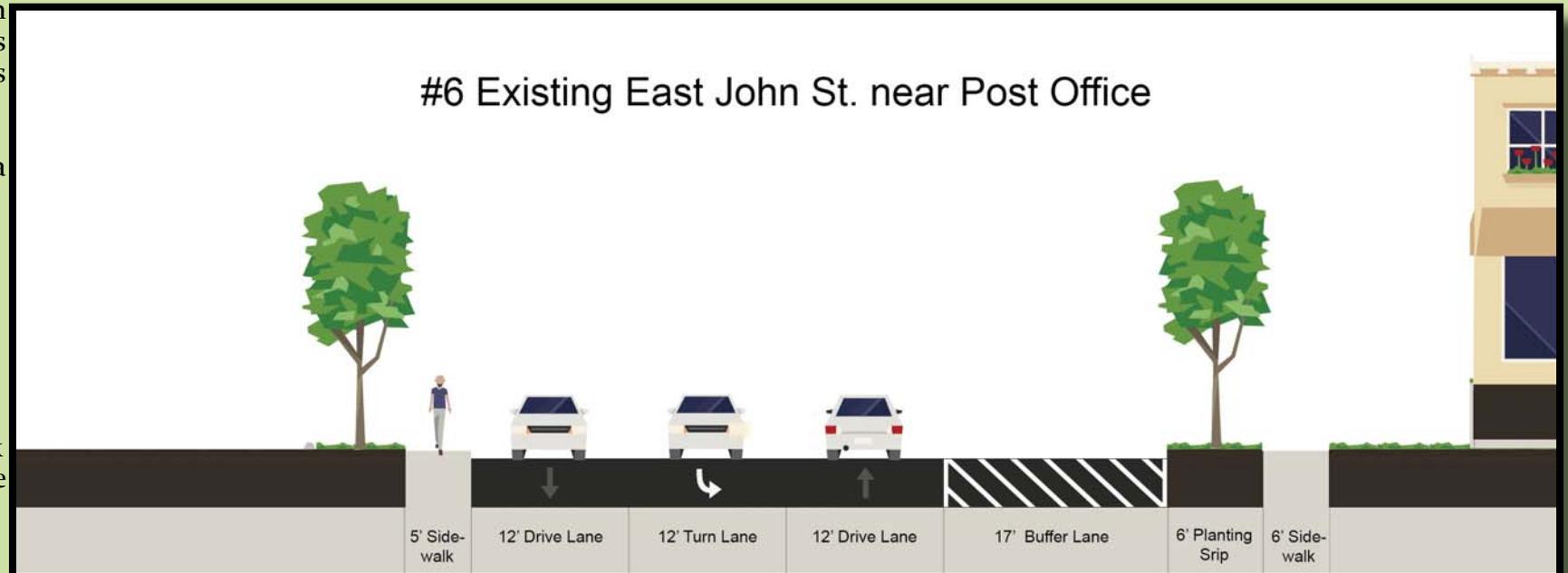


# DOWNTOWN STREETSCAPE IMPROVEMENT PLAN

## East John Street near Post Office - From Buckley Way to BB&T (Looking East)

The proposed East John Street near the Post Office cross section shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Unified Development Ordinance.
2. There shall be a clear, accessible sidewalk of a minimum of 5 feet in an 15-foot zone (North side) and a sidewalk of a minimum of 5 feet in width in a 11-foot zone (South side); that includes the curb, planting strip and building setback. Appurtenances can protrude 1 foot into the walk width and may include typical streetscape lighting, signage and furnishings as approved by the Town of Matthews.
3. No on-street parking will be permitted
4. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
5. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.



# DOWNTOWN STREETScape IMPROVEMENT PLAN

## McDowell Street (East/West)

The proposed McDowell Street cross section shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Development Codes.
2. There shall be a clear, accessible sidewalk of 5 feet in width, in an 11-foot amenity zone (on the Northeast side of the street), that includes the curb, planting strip and building setback. Within this 11' amenity zone, street trees street lighting, landscaping and/or furnishings may be permitted as approved by the Town of Matthews.
3. On the Southwest side of the street there shall be a minimum 10-foot sidewalk and 5-foot planting strip provided.
4. On street parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules or more details.
5. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
6. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.

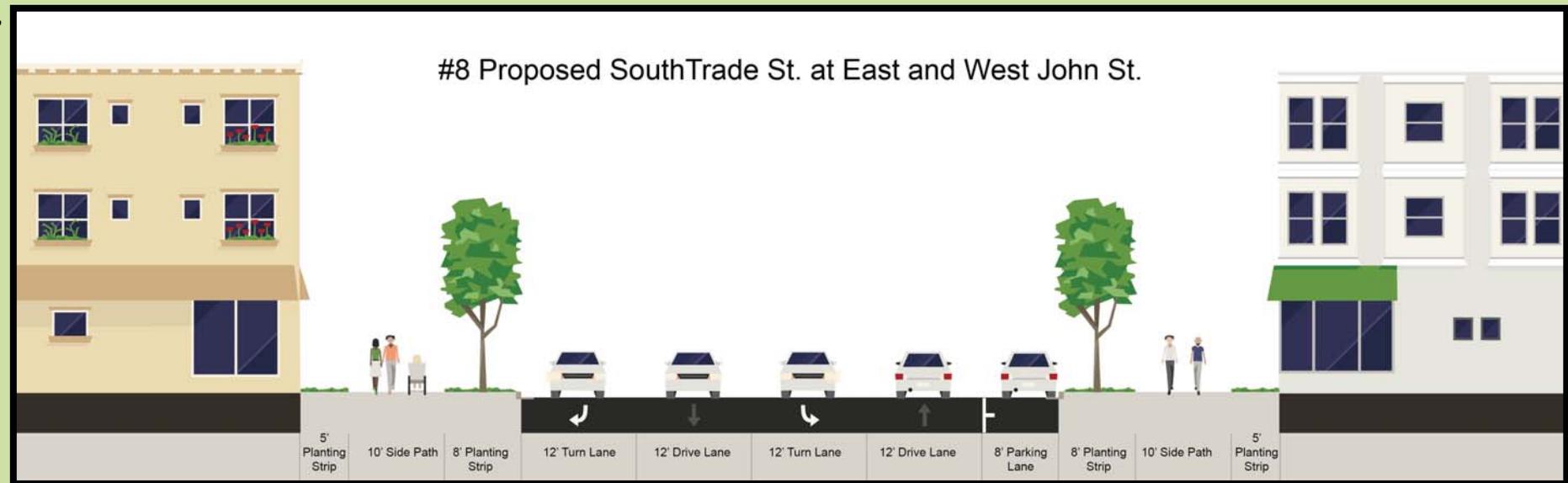
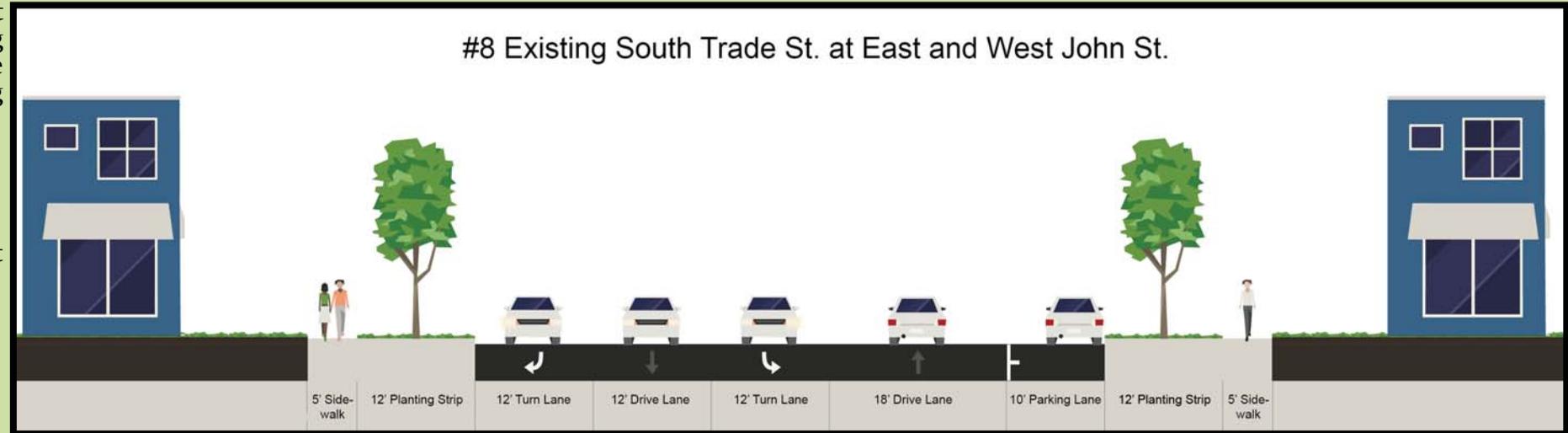


# DOWNTOWN STREETScape IMPROVEMENT PLAN

## South Trade Street at East and West John Street

The proposed South Trade Street at East and West John Street cross section shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Development Codes.
2. There shall be a clear, accessible multi-use path of 10 feet in width, in an 23-foot zone that includes the curb, planting strip and building setback.
3. There shall be a minimum 8-foot planting strip provided between the multi-use path and travel lane. This amenity zone may consist of street trees, street lighting, landscaping and/or furnishings.
4. On street parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules or more details.
5. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
6. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.

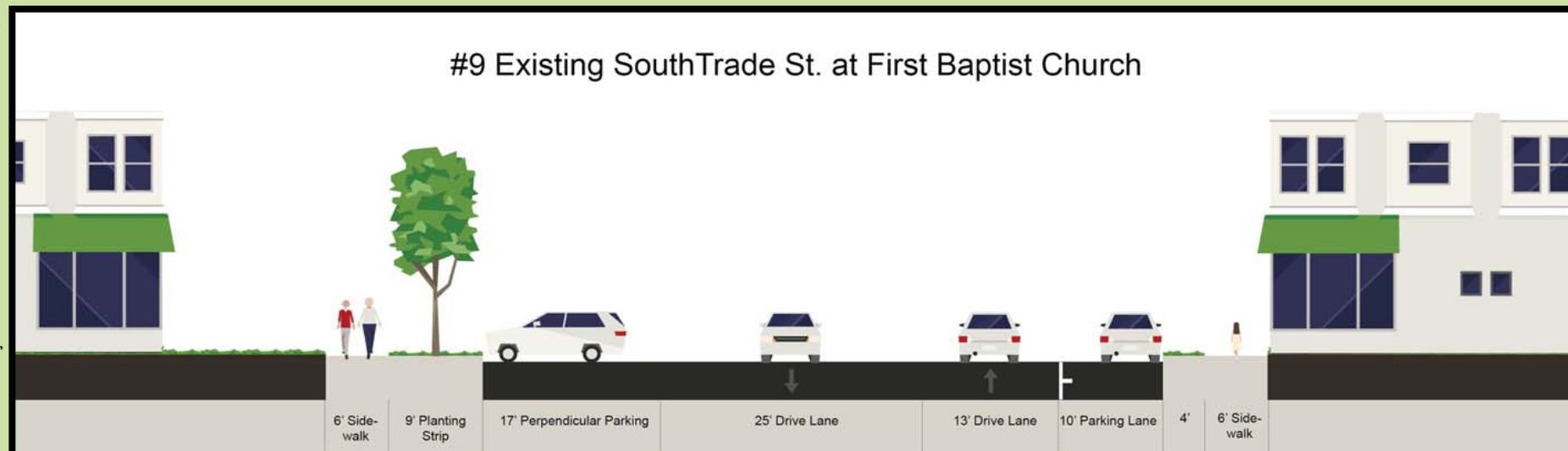


# DOWNTOWN STREETScape IMPROVEMENT PLAN

## South Trade Street at First Baptist Church (Looking South)

The proposed South Trade Street at First Baptist Church cross section shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Development Codes.
2. There shall be a clear, accessible multi-use path of 10 feet in width, in an 19-foot zone (on the South-east side of the street), that includes the curb, planting strip and building setback.
3. On the Northwest side of the street there shall be a minimum 6-foot sidewalk and 6-foot planting strip provided. Within this 12' amenity zone, street trees street lighting, landscaping and/or furnishings may be permitted as approved by the Town of Matthews.
4. On street parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules or more details.
5. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
6. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.



# DOWNTOWN STREETScape IMPROVEMENT PLAN

## South Trade Street at First Baptist Church (Looking North)

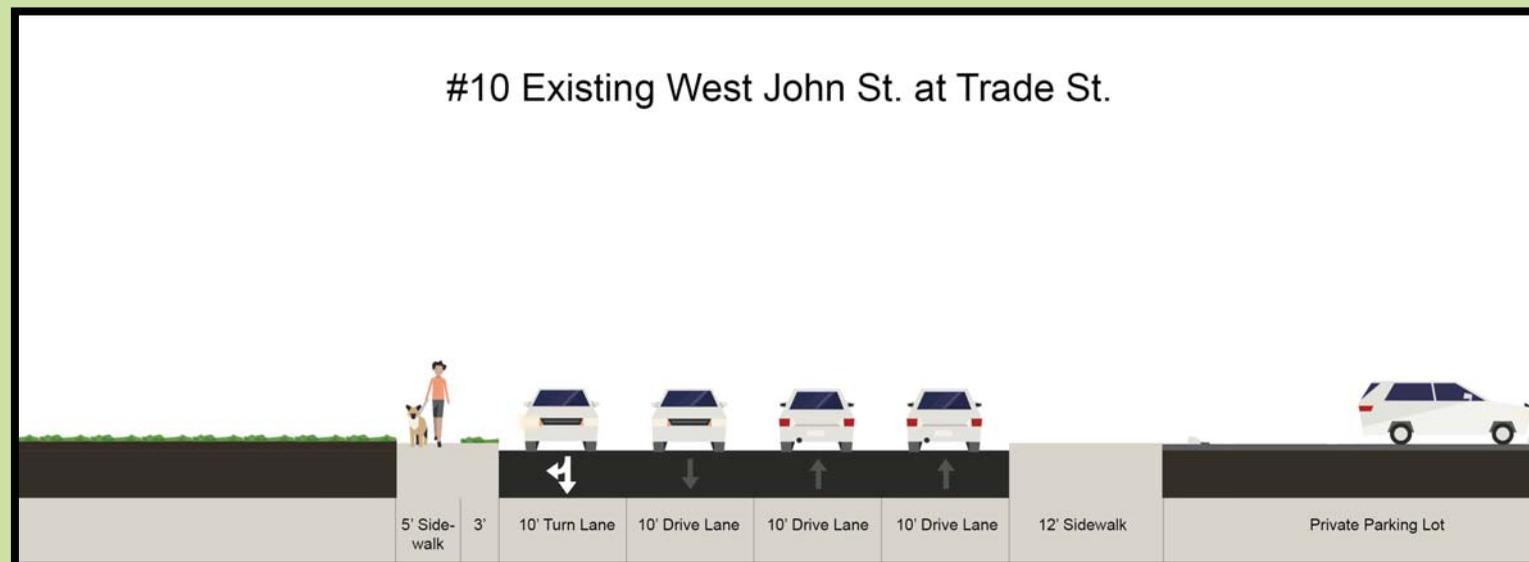


# DOWNTOWN STREETScape IMPROVEMENT PLAN

## West John Street at Trade Street (Option 10A)

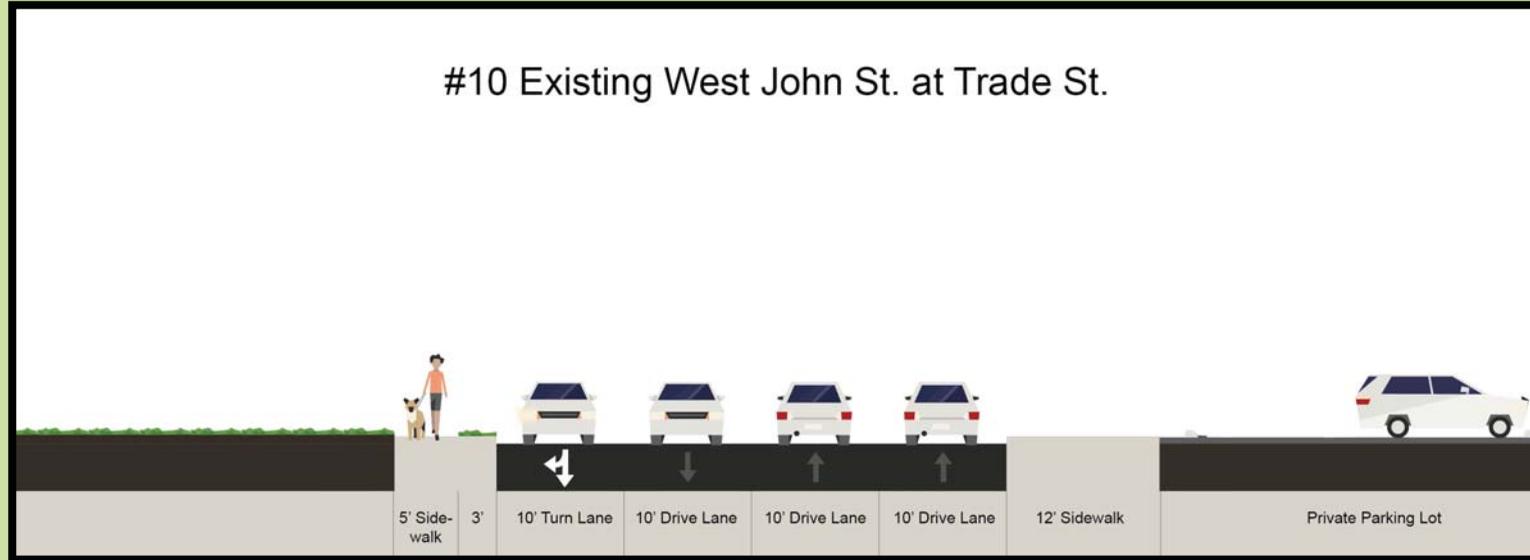
The proposed West John Street at Trade Street cross section shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. Parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules for more details.
2. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.



# DOWNTOWN STREETSCAPE IMPROVEMENT PLAN

## West John Street at Trade Street (Option 10B)



# DOWNTOWN STREETScape IMPROVEMENT PLAN

## Trade Street and John Street

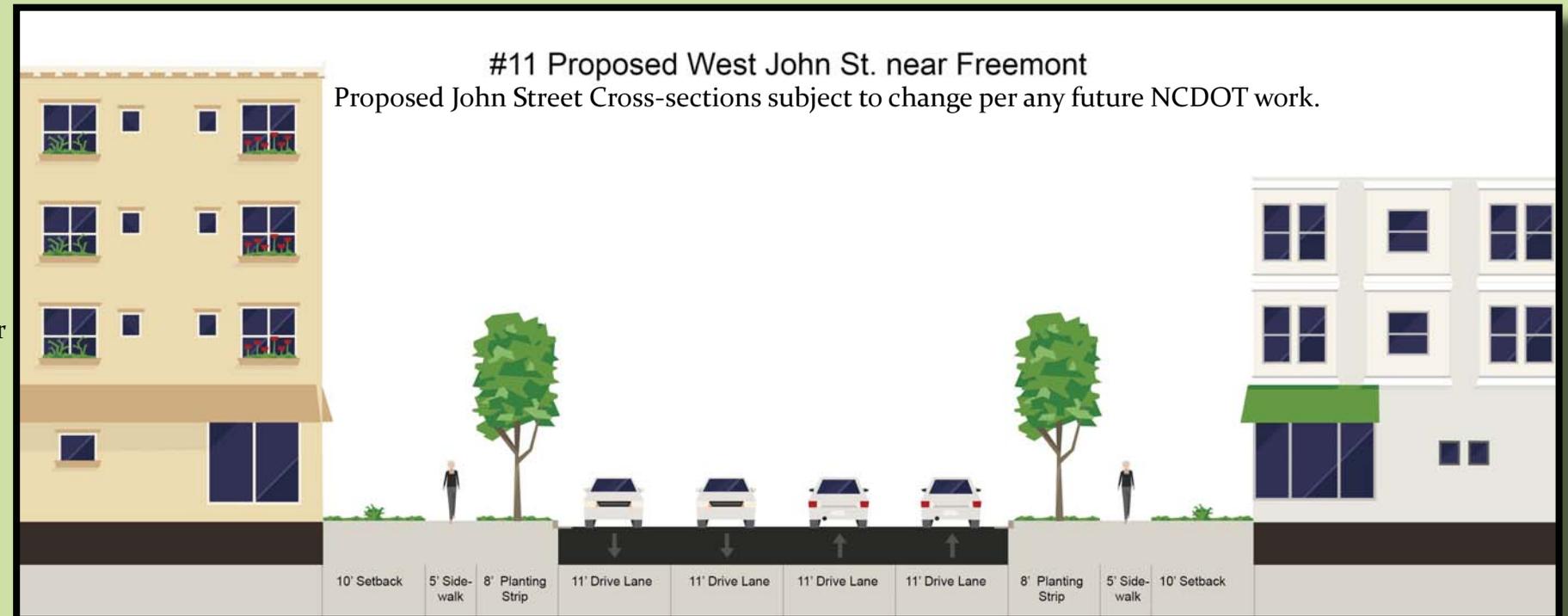
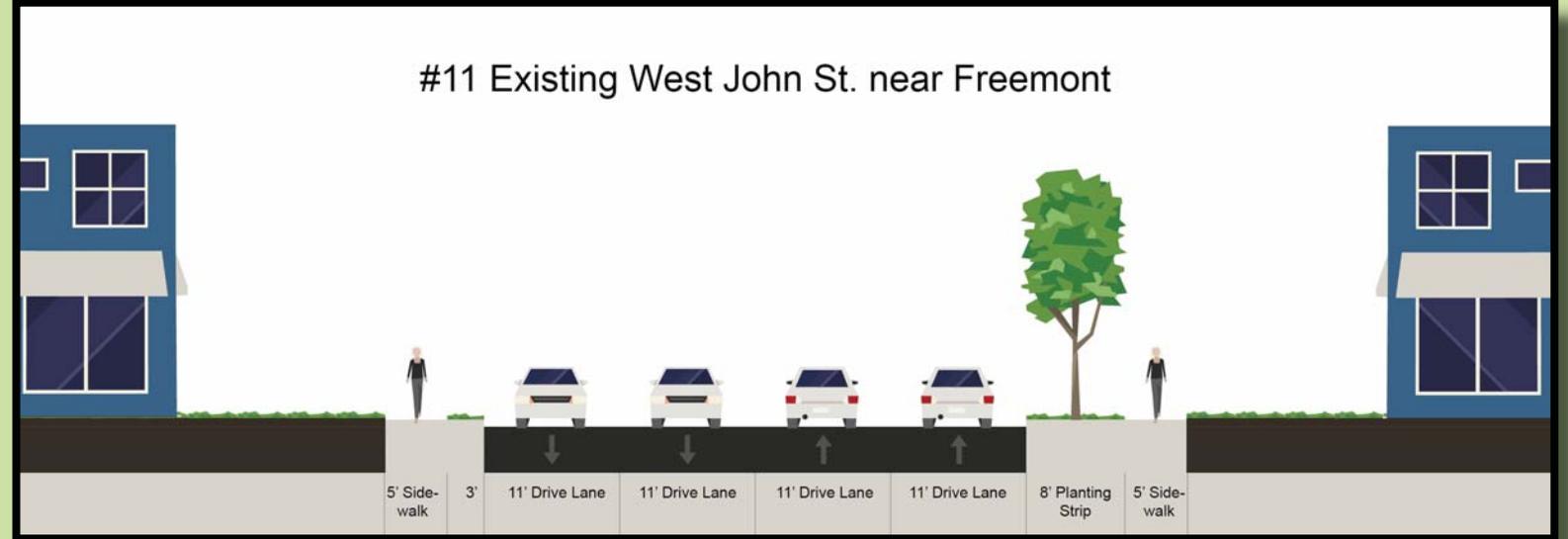


# DOWNTOWN STREETSCAPE IMPROVEMENT PLAN

## West John Street Near Freemont Street

The proposed West John Street near Freemont Street cross section shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Unified Development Ordinance.
2. There shall be a clear, accessible sidewalk of a minimum of 5 feet in an 23-foot zone that includes the curb, planting strip and building setback. Appurtenances can protrude 1 foot into the walk width and may include typical streetscape lighting, signage and furnishings as approved by the Town of Matthews.
3. No on-street parking will be permitted
4. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
5. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.

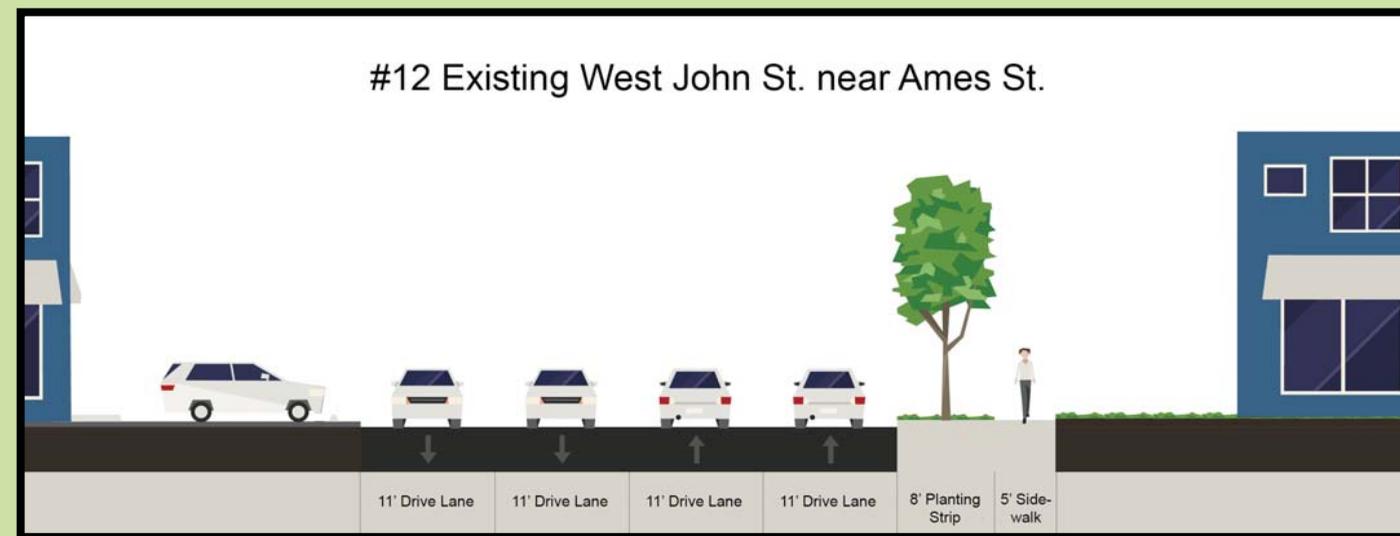


# DOWNTOWN STREETScape IMPROVEMENT PLAN

## West John Street Near Ames Street

The proposed West John Street near Ames Street cross section shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Development Codes.
2. There shall be a clear, accessible sidewalk of a minimum of 5 feet in an 23-foot zone that includes the curb, planting strip and building setback. Appurtenances can protrude 1 foot into the walk width and may include typical streetscape lighting, signage and furnishings as approved by the Town of Matthews.
3. No on-street parking will be permitted
4. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
5. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.



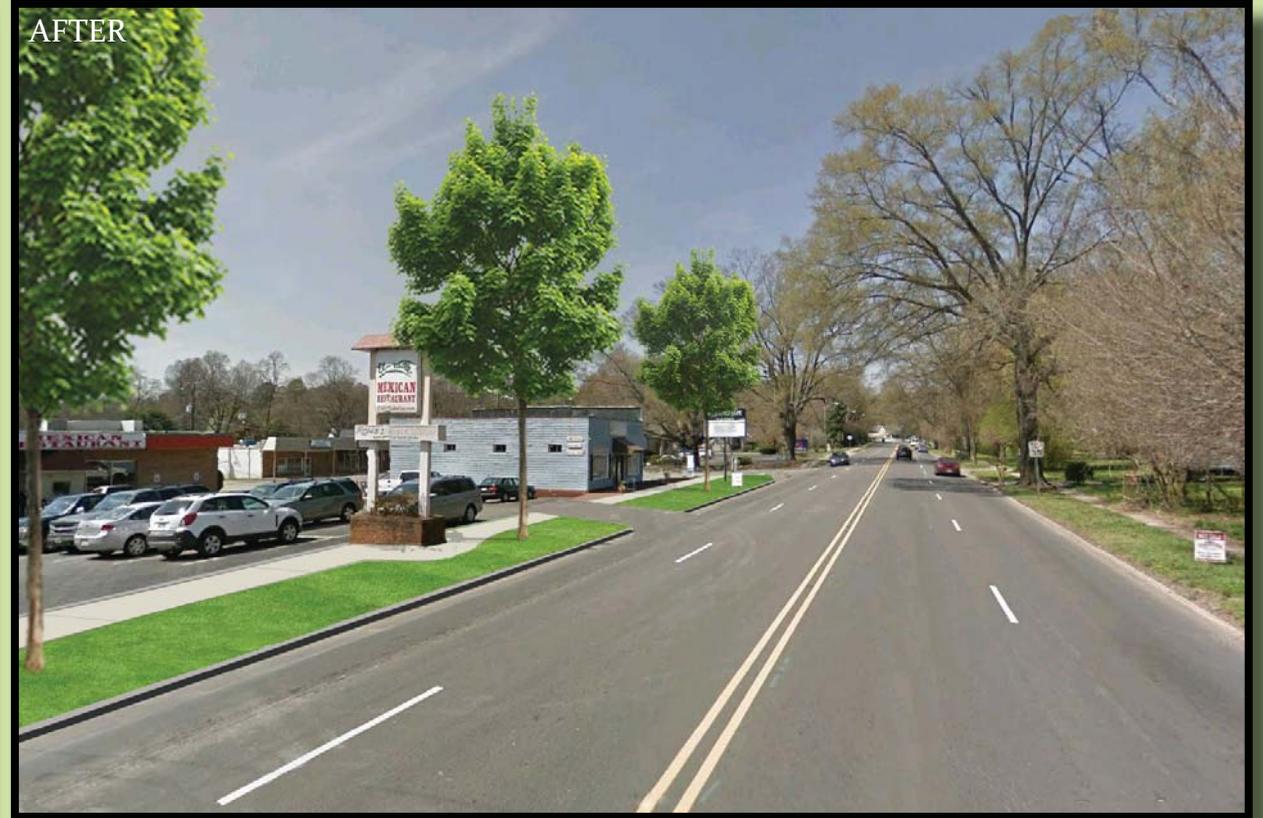
# DOWNTOWN STREETSCAPE IMPROVEMENT PLAN

## West John Street between Ames Street and Irwin Lane (Looking West)

BEFORE



AFTER

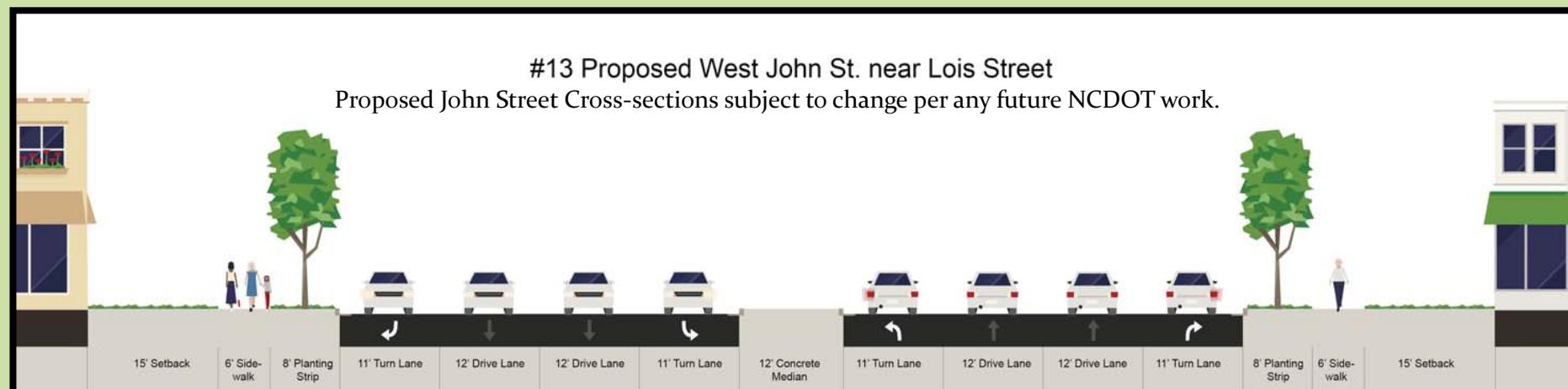
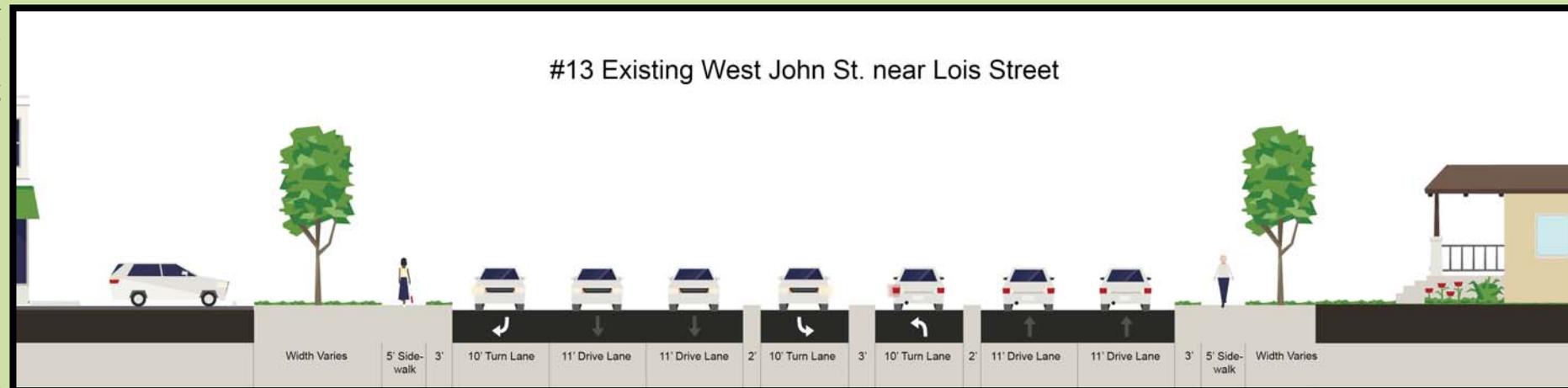


# DOWNTOWN STREETScape IMPROVEMENT PLAN

## West John Street near Lois Street (Looking North)

The proposed West John Street near Lois Street cross section shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

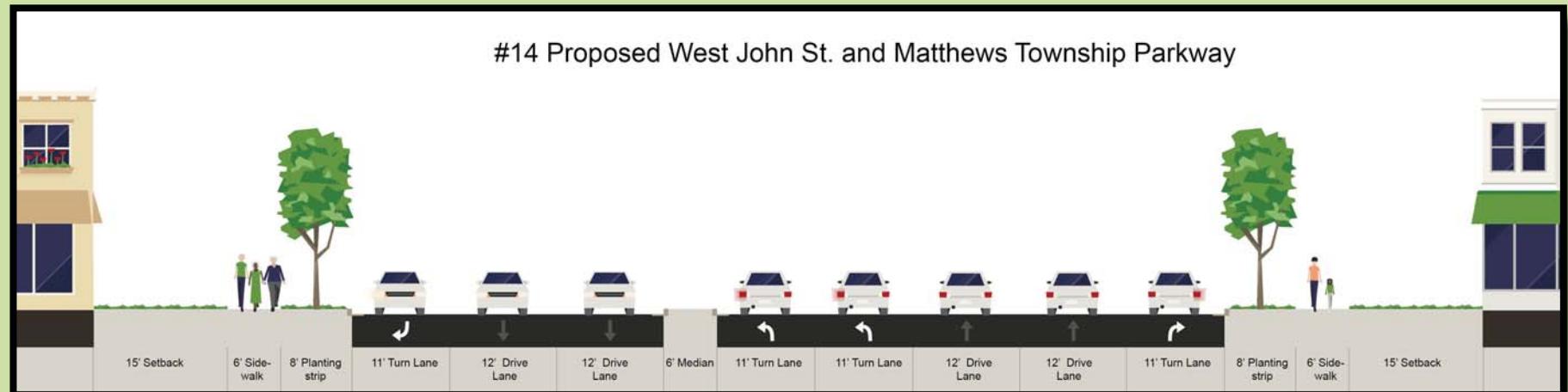
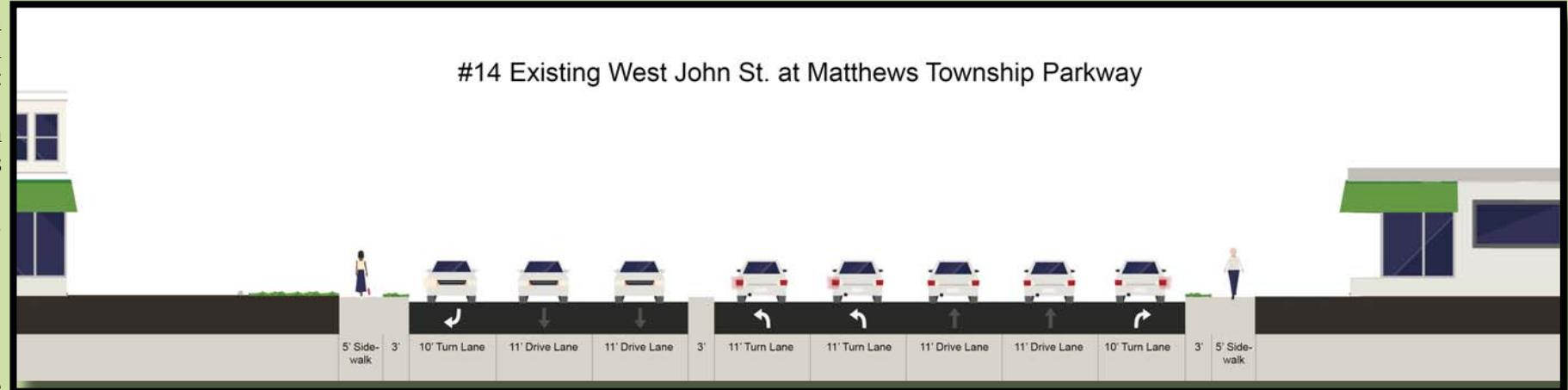
1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Unified Development Ordinance.
2. There shall be a clear, accessible sidewalk of a minimum of 6 feet in an 29-foot zone that includes the curb, planting strip and building setback. Appurtenances can protrude 1 foot into the walk width and may include typical streetscape lighting, signage and furnishings as approved by the Town of Matthews.
3. No on-street parking will be permitted
4. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
5. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.



## West John Street at Matthews Township

The proposed West John Street at Matthews Parkway Street cross section shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Unified Development Ordinance.
2. There shall be a clear, accessible sidewalk of a minimum of 6 feet in an 29-foot zone that includes the curb, planting strip and building setback. Appurtenances can protrude 1 foot into the walk width and may include typical streetscape lighting, signage and furnishings as approved by the Town of Matthews.
3. No on-street parking will be permitted
4. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
5. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.



# DOWNTOWN STREETScape IMPROVEMENT PLAN

## WEST JOHN STREET AND MATTHEWS TOWNSHIP PARKWAY

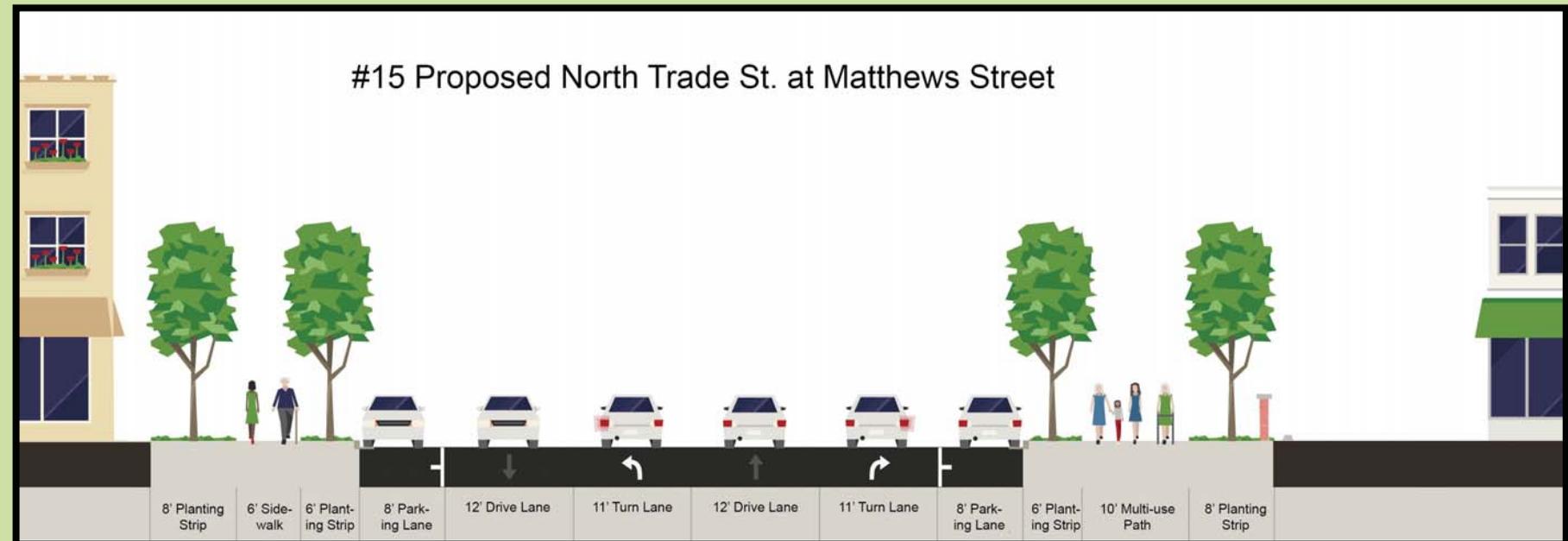
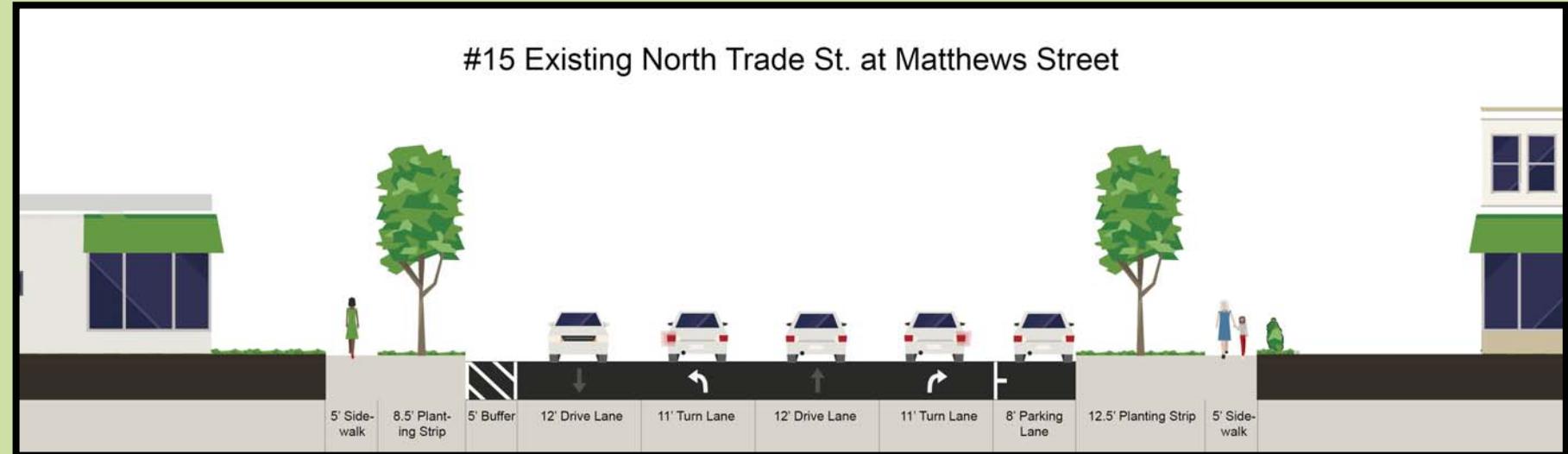


# DOWNTOWN STREETScape IMPROVEMENT PLAN

## Trade Street at Matthews Street (Looking North)

The proposed Trade Street at Matthews Street cross section shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Development Codes.
2. On the East side of the street there shall be a clear, accessible multi-use path of 10 feet in width, in an 44-foot zone that includes the curb, planting strip and building setback.
3. There shall be a clear, accessible sidewalk of 6 feet in width, in an 40-foot zone (on the North side of the street), that includes the curb, planting strip and bulding setback.
4. On street parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules or more details.
5. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
6. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.



# DOWNTOWN STREETScape IMPROVEMENT PLAN

## North Trade Street Near Park Center Drive (Looking North)

The proposed typical Trade Street cross section shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. Developers shall comply with any applicable North Carolina Department of Transportation regulations and standards that may be found elsewhere in the Town of Matthews Development Codes.
2. There shall be a clear, accessible multi-use path of 10 feet in width, in an 28-foot zone (on the South-east side of the street), that includes the curb, planting strip and bulding setback.
3. There shall be a clear, accessible sidewalk of 6 feet in width, in an 24-foot zone (on the North-west side of the street), that includes the curb, planting strip and building setback.
4. No on-street parking shall be permitted. Parking may only be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules or more details.
5. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
6. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.

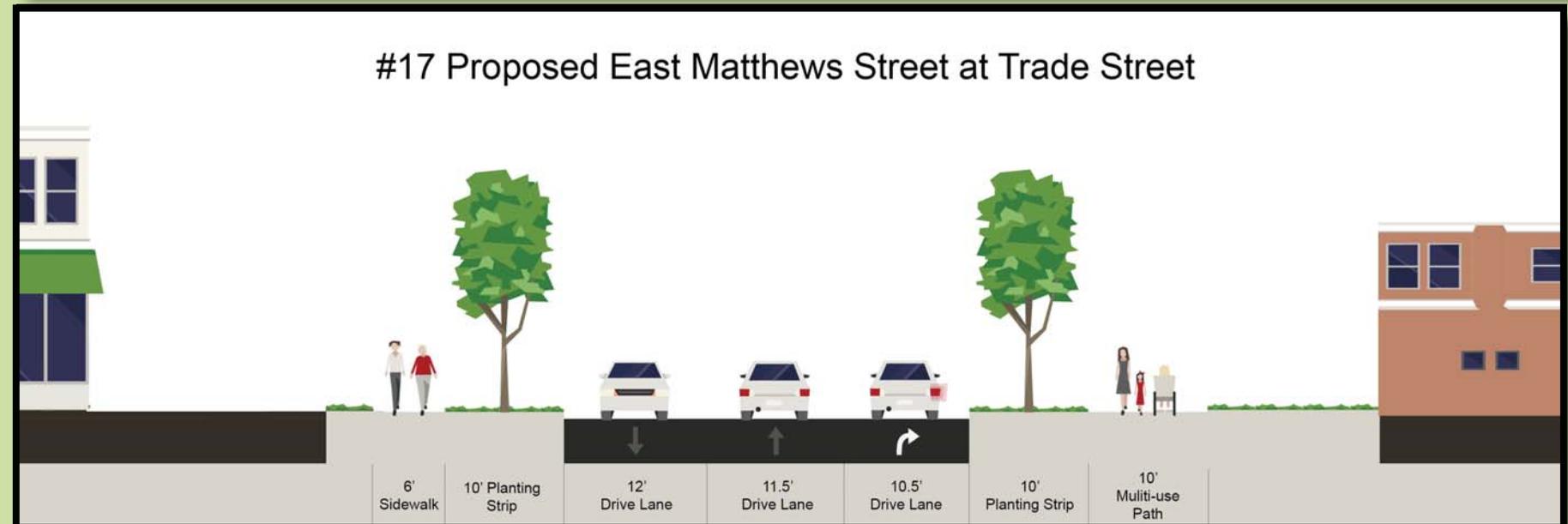
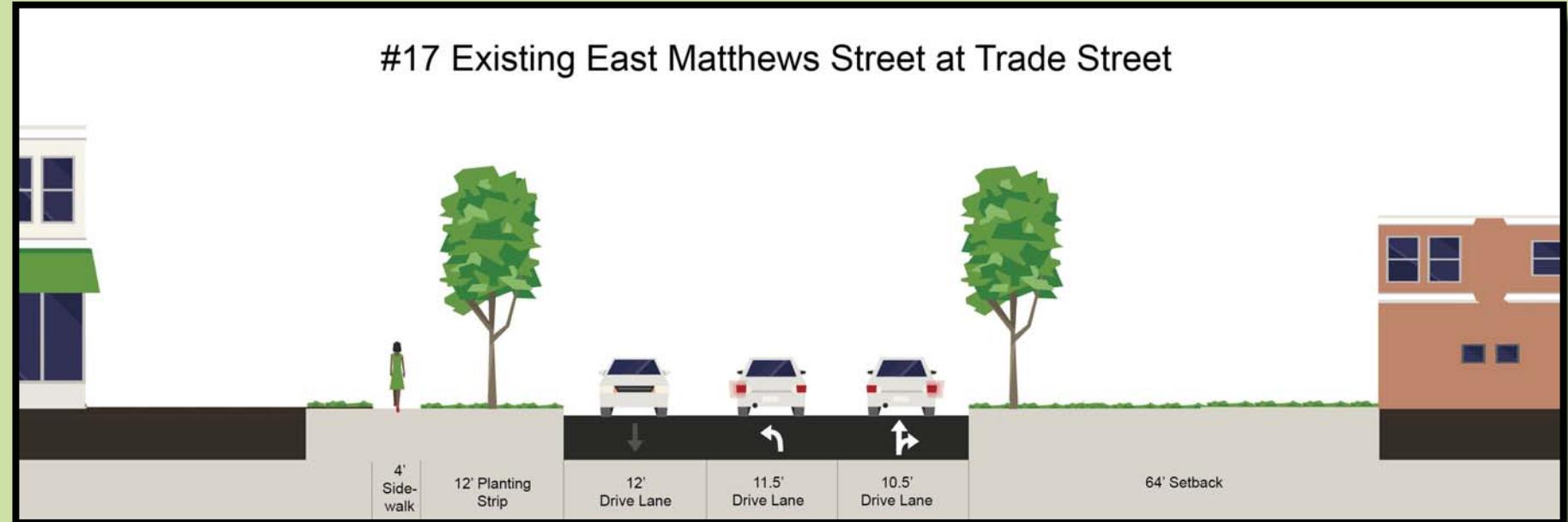


# DOWNTOWN STREETScape IMPROVEMENT PLAN

## East Matthews Street at Trade Street (Looking West)

The proposed Matthews Street at Trade Street cross section shall meet the following minimum Street Type Standards unless otherwise indicated in this or other Town of Matthews planning documents:

1. There shall be a clear, accessible multi-use path of 10 feet in width, in an 20-foot zone (on the North side of the street), that includes the curb, planting strip and building setback.
2. There shall be a clear, accessible sidewalk of 6 feet in width, in an 16-foot zone (on the South side of the street), that includes the curb, planting strip and building setback.
3. On street parking will be permitted according to Town of Matthews Ordinances only. See Town Ordinances, Chapter 74, Parking Schedules or more details.
4. Street lamp lighting should be located within the planting strip and shall match the standard for Downtown Matthews as illustrated in the Matthews Downtown Plan.
5. No new driveways are permitted without permission of the Town of Matthews and shall be allowed only in instances that permit no other options.



# DOWNTOWN STREETScape IMPROVEMENT PLAN

## Photographic Representations (Before & After)

The following “Before and After” images provide a graphical representation of how new and existing streets could be developed or redeveloped in the future. Many different streetscape elements such as sidewalks, multipurpose paths, traffic calming, street lighting, landscaping, buildings and parking configurations are explored.

The intent of the photographic representations is not to dictate specific design expectations or configurations but instead are meant to relay design possibilities intended to encourage an attractive and successful public realm.

### North Trade Street and East Matthews Street (1)



# DOWNTOWN STREETScape IMPROVEMENT PLAN

## North Trade Street and East Matthews Street (2)



# DOWNTOWN STREETScape IMPROVEMENT PLAN

## North Trade Street and East Matthews Street (3)

BEFORE



AFTER



# DOWNTOWN STREETSCAPE IMPROVEMENT PLAN

## North Trade Street and East Matthews Street (4)



# DOWNTOWN STREETScape IMPROVEMENT PLAN

## Proposed fountain on Matthews Station Street



# DOWNTOWN STREETScape IMPROVEMENT PLAN

## Alternative Transportation Paths Map (Walking Paths, Cycling and Bus Routes)



Bus equipped to accommodate cyclists needs



Bus stop location with bike rack

As the Town grows so will the need to provide pedestrian amenities. The map to the right displays existing and future alternative transportation routes.

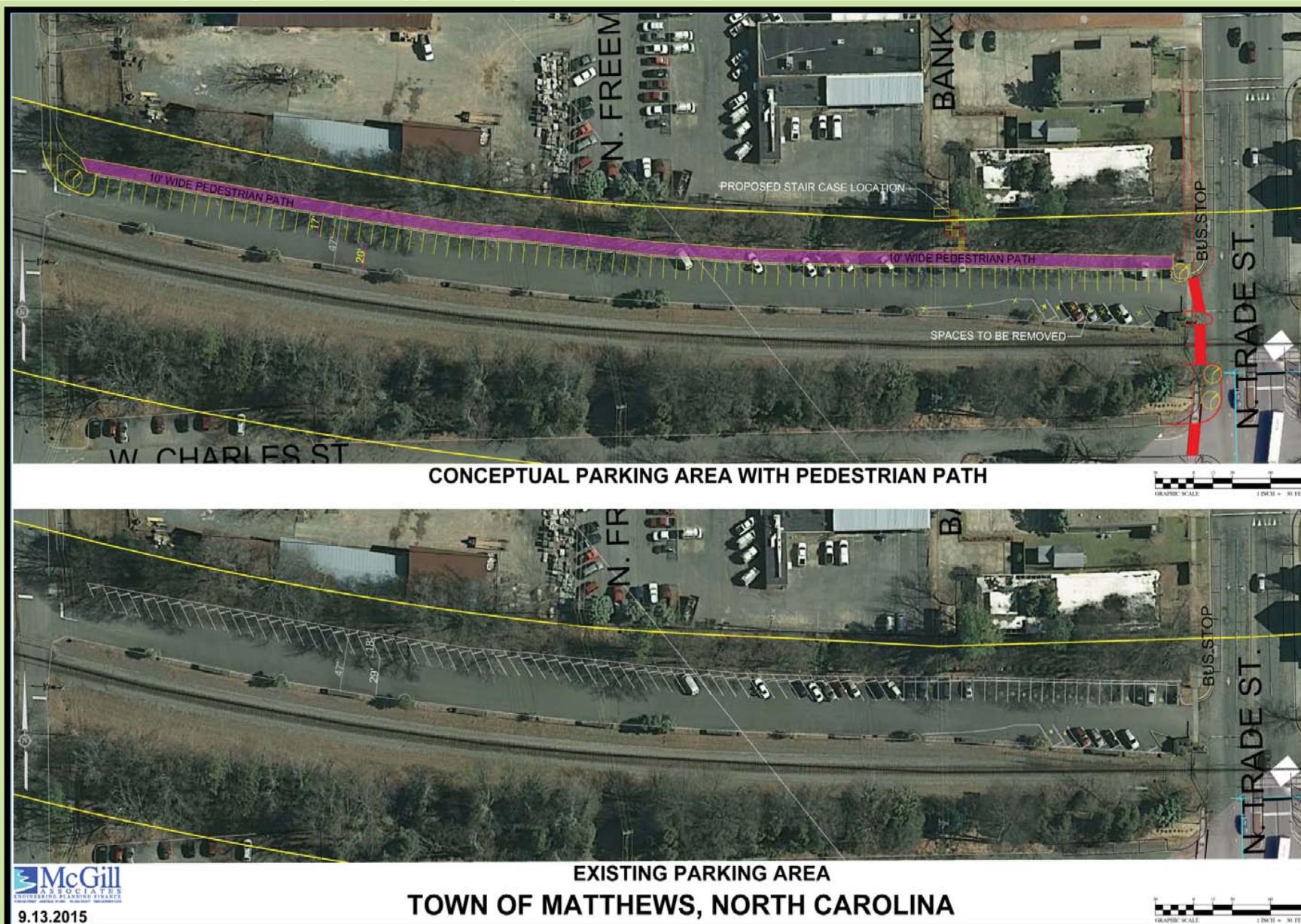
The town should follow the Matthews Composite Bicycle and Pedestrian Plan.



# DOWNTOWN STREETScape IMPROVEMENT PLAN

## Proposed pedestrian path through parking lot between Trade Street and Ames Street

A conceptual sketch of how a revision to the parking lot layout may provide space for a 10' wide pedestrian walk can be seen below.



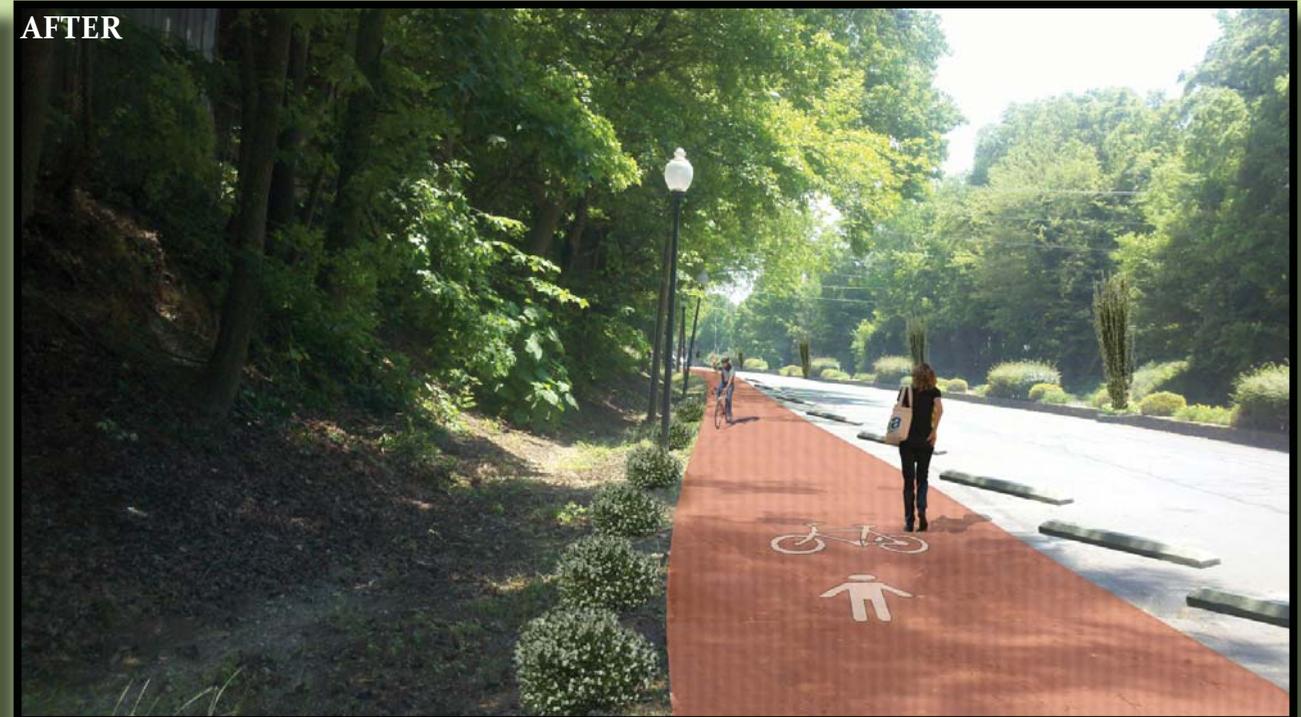
# DOWNTOWN STREETScape IMPROVEMENT PLAN

**Proposed pedestrian path through parking lot between Trade Street and Ames Street**

**BEFORE**



**AFTER**



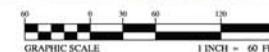
# DOWNTOWN STREETScape IMPROVEMENT PLAN

## NCDOT proposed improvements for John Street

A conceptual sketch of John Street has been provided by NCDOT. At the time this document was written, NCDOT's plans had not been confirmed and alternative iterations of the planned widening of John street were expected. This plan proposes a widened center median with a minimum width of 15' to accommodate street trees, and pedestrian refuge (see Option 7A Below).



**OPTION A (NCDOT CONFIGURATION AS OF 9.9.15)**  
5' WIDE PAVED MEDIAN WITH NO SEPARATION BETWEEN SIDEWALK AND TRAVEL LANES



**OPTION B**  
ALTERNATIVE CONCEPT WITH 15' WIDE PLANTED MEDIAN

